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Anthony Washington
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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON, III, P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2, 2-1
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1, 2B2
GENERAL NOTES.....	2C
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PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2026	19S155-S8-013	ROADWAY-SIGN 1

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**SIGNATURE
SHEET**

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DAVIDSON COUNTY

STATE ROUTE 155
FROM SR-6 (FRANKLIN PIKE)
TO NEAR I-24 (GTF-26)
RESURFACE & SAFETY
MILL, B-M2, 411D AND PAVEMENT MARKING

STATE HIGHWAY NO. 155 F.A.H.S. NO. N/A

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2026	1
STATE PROJ. NO.	19S155-S8-013	
STATE PROJ. NO.	19S155-M3-013	
STATE PROJ. NO.	19047-2243-04	



PROJECT LOCATION
BRIDGE ID. # 19100650011 19SR1550003 19SR1550005

END PROJECT NO. 19S155-S8-013 RESURFACE & SAFETY
L.M. 2.80

CSX OVERHEAD CROSSING #350244Y
SR-155 L.M. 1.26
CSX M.P. (OBA 0189.350)
LAT: 36.1112640 LONG: -86.7512750

END PROJ. NO. 19047-2243-04 R.O.W.
STA 19+78.16
N 648321.5873
E 1741121.1806

CSX UNDER CROSSING #350209K
SR-155 L.M. 0.11
CSX M.P. (000 0190.040)
LAT: 36.1116060 LONG: -86.7704720

BRIDGE DECK REPAIR PROJECT NO. 19S155-M3-013
SR-155 @ L.M. 0.11

BEGIN PROJ. NO. 19047-2243-04 R.O.W.
STA 17+32.49
N 648155.1456
E 1740940.9061

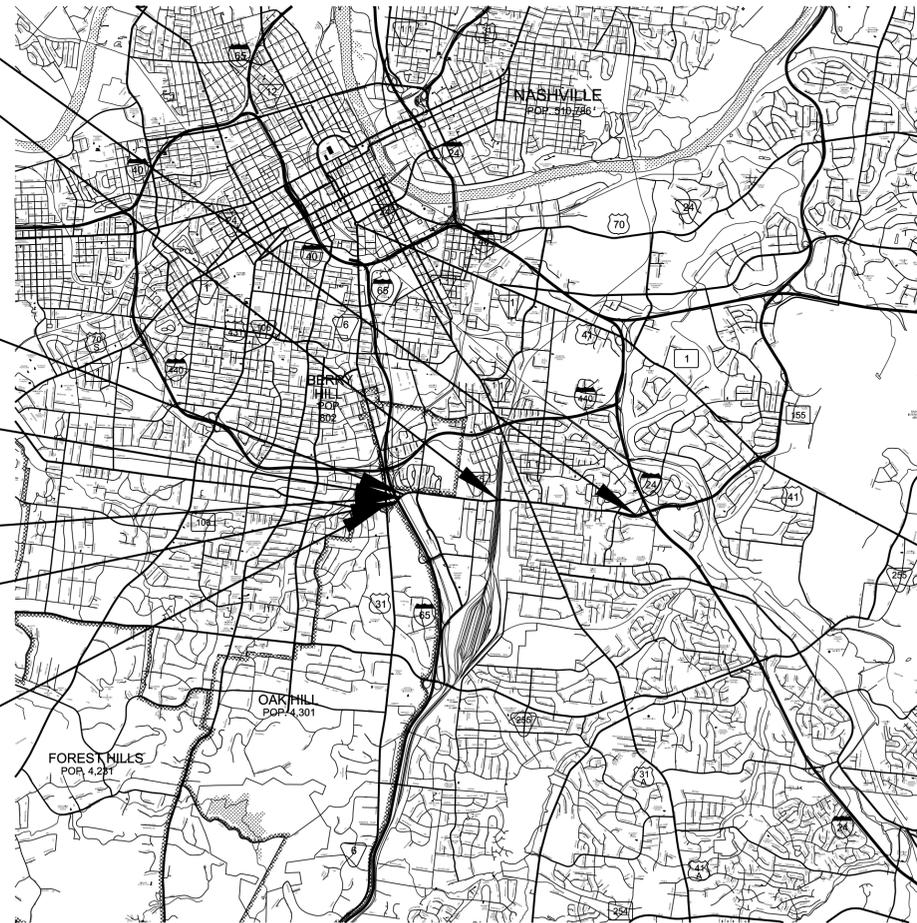
BEGIN PROJECT NO. 19S155-S8-013 RESURFACE & SAFETY
L.M. 0.00

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: KONNER SPRADLIN, P.E., REGION 3
DESIGN FIRM : HDR ENGINEERING, INC.
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.
P.E. NO. 98034-4299-04 (DESIGN)
PIN NO. 129489.00



SCALE: 1"= 5280'



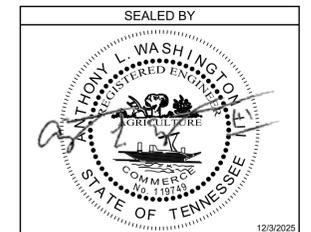
PROJECT LENGTH 2.72 MILES
TOTAL LANE MILES RESURFACED 14.47 MILES



EXCLUSIONS	
L.M. TO L.M.	LENGTH (FT.)
1.25 - 1.33	422.4
-	-
-	-
TOTAL =	422.4

TRAFFIC COUNTER, EMBEDDED DETECTION LOOPS & AUTOMATIC TRAFFIC READERS	
STATION LOCATION	LOG MILE
TC STATION 109	0.271
TC STATION 107	1.404
TC STATION 105	1.726
TC STATION 319	2.678

TRAFFIC DATA	
ADT (2026)	30043
POSTED SPEED LM 0.00 - 2.59	40 MPH
POSTED SPEED LM 2.59 - 2.80	45 MPH



APPROVED:
SHANE HESTER, P.E., CHIEF ENGINEER

DATE:

APPROVED:
WILL REID, P.E., COMMISSIONER

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2, 2-1
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1, 2B2
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PAVEMENT MARKING IMPROVEMENTS.....	2G
PAVEMENT MARKING IMPROVEMENTS AND SIGNING PLAN	2G1 – 2G13
CURB RAMP IMPROVEMENTS	2G14 – 2G15
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3, 3-1
PROPERTY MAP AND RIGHT-OF-WAY ACQUISITION TABLE	3A
PRESENT LAYOUT.....	4
RIGHT-OF-WAY DETAILS	4A
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1
BRIDGE PLANS	B1
NO UTILITY SHEETS ARE INCLUDED IN THIS SET OF PLANS	

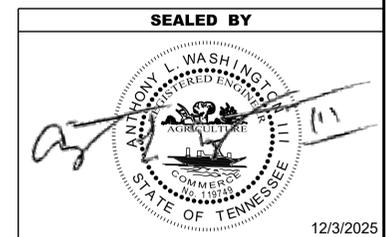
STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
10-107.00 MULTIMODAL		
CR-GN-1		GENERAL NOTES CURB RAMP
CR-GN-2		CURB RAMP NUMBERING
CR-DWS-1		DETECTABLE WARNING SURFACE DETAIL
CR-GTL-1		GUTTER TRANSITION LAYOUT
CR-GTL-2		GUTTER TRANSITION LAYOUT
CR-10		PERPENDICULAR CURB RAMP
CR-20	07-01-25	PARALLEL CURB RAMP 5 – FT SIDEWALK
CR-21		COMBINATION RAMP USED WITH WIDE SIDEWALK
MM-PM-2	07-30-24	SIGNING AND PAVEMENT MARKINGS FOR BICYCLE LANE OR ROUTES
MM-PM-3	06-15-21	SIGNING AND PAVEMENT MARKINGS FOR BICYCLE LANES ON URBAN ROADWAYS
MM-PM-4		SIGNING AND PAVEMENT MARKINGS FOR BICYCLE LANES AT INTERSECTIONS
MM-PM-5	05-04-22	SIGNING AND PAVEMENT MARKINGS FOR BICYCLE LANES AT INTERSECTIONS
MM-TS-1	06-15-21	BIKE ACCOMMODATION DESIGN GUIDANCE
10-108.00 SAFETY DESIGN AND GUARDRAILS		
S-CZ-1	06-28-19	CLEAR ZONE CRITERIA

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-200.00 SIGN		
T-S-12	07-30-25	STANDARD STEEL GROUND MOUNTED SIGNS, BREAK-AWAY TYPE POST FOOTING DETAILS, SQUARE TUBES
T-S-16	07-30-25	GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-S-17	07-30-25	STANDARD GROUND MOUNTED SIGN USING PERFORATED/KNOCKOUT SQUARE TUBE
T-S-19	07-30-25	STANDARD STEEL SIGN SUPPORTS
T-S-20	07-30-25	SIGN DETAILS
T-S-23A	07-30-25	MULTI-DIRECTIONAL SLIP BASE BREAKAWAY P-POST SIGN SUPPORT
10-201.00 SIGNALS		
T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
10-204.00 DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-18	01-24-25	FLEXIBLE DELINEATOR DETAILS
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-26-25	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-22		ONE LANE CLOSURE DETAIL FOR UNDIVIDED HIGHWAYS
T-WZ-40	03-26-25	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-41	03-26-25	LEFT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-55	03-26-25	SIDEWALK TRAFFIC CONTROL
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	1A



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ROADWAY INDEX,
STANDARD ROADWAY
DRAWINGS AND
STANDARD TRAFFIC
DESIGN DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 19I00650011 SR-155 OVER I-65 AND CSXRR LM 0.11 (19-I0065-00.11). THE BRIDGE HAS 7300 SQUARE FEET OF TEXTURE COATINGS ON BENTS, BENT CAPS, AND ABUTMENTS AT 2% CHRYSOTILE, AND 4500 LINEAR FEET OF TRANSITE UTILITY PANELS AT 18% CHRYSOTILE AND 2% CROCIDOLITE. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS.	BRIDGE NO. 19I00650011 SR-155 OVER I-65 AND CSXRR LM 0.11 (19-I0065-00.11)
EDHZ002	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	THE STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TDEC RULES CHAPTER 1200-01-20) MANDATES THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS. ABATEMENT OF THIS MATERIAL SHALL BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. ACM ABATEMENT SHOULD BE COMPLETED PRIOR TO ANY DEMOLITION ACTIVITIES IF POSSIBLE. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	

SEALED BY

12/3/2025

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**PROJECT
COMMITMENTS**

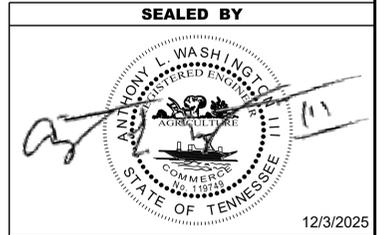
ESTIMATED ROADWAY QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2

ITEM NO.	DESCRIPTION	UNIT	QUANTITY 19S155-S8-013	QUANTITY NON- PARTICIPATING	TOTAL QUANTITY
105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1		1
202-01.03	REMOVAL OF TRASH AND DEBRIS	LS	1		1
202-03	REMOVAL OF RIGID PAVEMENT, SIDEWALK, ETC.	S.Y.	39		39
202-08.15	REMOVAL OF CURB AND GUTTER (CONCRETE)	L.F.	154		154
(1) 203-07	FURNISHING & SPREADING TOPSOIL	C.Y.	6		6
(1)(2)(3) 208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	3.7		3.7
(4) 208-02.01	DITCH (RIPRAP, SAND & PLASTIC SHEETING)	L.F.	386		386
(1)(2) 303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	243		243
(5) 307-01.15	ASC MIX (PG64-22) (BPMLC-HM) GRADING CS	TON	200		200
(6) 307-02.08	ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING B-M2	TON	13804		13804
(7) 403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	105		105
(8) 411-01.21	LONGITUDINAL JOINT SEALANT	L.M.	12.5		12.5
(9) 411-02.10	ACS MIX(PG70-22) GRADING D	TON	7976		7976
(10) 415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	20671		20671
(11) 611-01.20	ADJUSTMENT OF EXISTING MANHOLE	EACH	3		3
(12) 611-09.01	ADJUSTMENT OF EXISTING CATCHBASIN	EACH	10		10
701-01.01	CONCRETE SIDEWALK (4 ")	S.F.	348		348
701-01.02	CONCRETE SIDEWALK (6 ")	S.F.	148		148
701-02.01	CONCRETE CURB RAMP (RETROFIT)	S.F.	899		899
701-02.03	CONCRETE CURB RAMP	S.F.	750		750
701-02.06	DETECTABLE WARNING SURFACE (REHABILITATION)	S.F.	80		80
702-01	CONCRETE CURB	C.Y.	3		3
702-03	CONCRETE COMBINED CURB & GUTTER	C.Y.	7		7
(13) 712-01	TRAFFIC CONTROL	LS	1		1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	75		75
(14) 712-06	SIGNS (CONSTRUCTION)	S.F.	1950		1950
713-02.11	REMOVAL OF EXISTING DELINEATORS	EACH	6		6
(15) 713-02.14	FLEXIBLE DELINEATOR (WHITE)	EACH		300	300
(15) 713-02.15	FLEXIBLE DELINEATOR (YELLOW)	EACH		34	34
(15) 713-11.02	PERFORATED/KNOCKOUT SQUARE TUBE POST	LB.	630		630
713-11.21	P POST SLIP BASE	EACH	7		7
(15) 713-13.02	FLAT SHEET ALUMINUM SIGNS (0.080" THICK)	S.F.	85		85
(15) 713-13.03	FLAT SHEET ALUMINUM SIGNS (0.100" THICK)	S.F.	130		130
713-15	REMOVAL OF SIGNS, POSTS AND FOOTINGS	LS	1		1
716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	288		288
716-01.22	SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	379		379
716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	118		118
716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	20		20
(16)(17) 716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	276		276
(16)(17)(18) 716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	1237		1237
(16)(17) 716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	61		61
(16)(17) 716-02.09	PLASTIC PAVEMENT MARKING (LONGITUDINAL CROSS-WALK)	L.F.	709		709
(16)(17) 716-02.11	PLASTIC PAVEMENT MARKING (6" DOTTED LINE)	L.F.	6250		6250
(16)(17) 716-02.12	PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	1		1
(16)(17) 716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	1		1
(16)(17) 716-03.07	PLASTIC WORD PAVEMENT MARKING (STOP)	EACH	2		2
(16)(17) 716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	95		95
(16)(17) 716-04.07	PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	3		3
(16)(17)(19) 716-04.10	PLASTIC PAVEMENT MARKING (HANDICAP SYMBOL)	EACH	13		13
(16)(17) 716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	348		348
(16)(17) 716-04.13	PLASTIC PAVEMENT MARKING (BIKELANE SYMBOL & ARROW)	EACH	39		39
(16)(17) 716-04.14	PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH	5		5
(16)(17) 716-04.17	PLASTIC PAVEMENT MARKING (YIELD SYMBOL)	EACH	2		2
(17) 716-04.21	GREEN TEXTURED BIKE LANE	S.Y.		460	460
(20) 716-05.04	PAINTED PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	276		276
(20) 716-05.05	PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	1237		1237
(20) 716-05.06	PAINTED PAVEMENT MARKING (TURN LANE ARROW)	EACH	61		61
(20) 716-05.22	PAINTED PAVEMENT MARKING (LONGITUDINAL CROSS-WALK)	L.F.	709		709
(20) 716-05.49	PAINTED PAVEMENT MARKINGS(8" LINE)	L.M.	1		1
(21) 716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	14.2		14.2
(22) 716-08.01	REMOVAL OF PAVEMENT MARKING (LINE)	L.F.	6284		6284

THERE ARE NO GUARDRAIL ADJUSTMENTS WITHIN THE PROJECT LIMITS

SEE SHEET 2-1 FOR FOOTNOTES



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**ESTIMATED
ROADWAY
QUANTITIES**

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY 19S155-S8-013	QUANTITY NON- PARTICIPATING	TOTAL QUANTITY
(23)	716-08.24 REMOVAL OF PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH	7		7
(24)	716-09.83 CONTRAST PAVEMENT MARKING 6"	L.F.	4858		4858
(25)	716-09.90 CONTRAST PAVEMENT MARKING 6" DOTTED	L.F.	728		728
(26)	716-09.97 CONTRAST PAVEMENT SHADOW MARKING 6"	L.F.	1056		1056
(17)	716-12.02 ENHANCED FLATLINE THERMO PVM T MRKNG (6IN LINE)	L.M.	13		13
	717-01 MOBILIZATION	LS	1		1
(27)	730-14.02 SAW SLOT	L.F.	3208		3208
(27)	730-14.03 LOOP WIRE	L.F.	7216		7216
	801-01 SEEDING (WITH MULCH)	UNIT	1		1
(15)	920-13.04 TUFF CURB XLP WITH TUBULAR MARKER (SUPPLY AND INSTALL)	EACH		103	103

THERE IS NO GUARDRAIL WORK ON THIS PROJECT

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER.
- (2) NO DIRT OR DEBRIS WILL BE LEFT ON THE SHOULDERS INCLUDING UNDER AND AROUND GUARDRAIL. SHOULDERS SHOULD BE CLIPPED AS DIRECTED BY THE ENGINEER TO ALLOW FOR PROPER DRAINAGE. THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING COLD PLANNING AND ASPHALT DEBRIS DOES NOT ENTER THE DRAINS AND WILL BE RESPONSIBLE FOR CLEAN OUTS.
- (3) INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, PAVEMENT, RESURFACING, NOTE 2 FOR MORE INFORMATION.
- (4) TO BE USED FOR REPAIR OF THE DITCH AT THE CEMETERY AT L.M. 0.74 TO 0.813 EASTBOUND.
- (5) TO BE USED FOR SPOT REPAIR.
- (6) INCLUDES 1028 TONS FOR COUNTY ROADS, BUSINESS ENTRANCES, DRIVEWAYS, TURN LANES, INTERSECTIONS, CROSS OVERS, HANDICAP PARKING AREA AND RAMPS. INCLUDES 200 TONS FOR SPOT REPAIR.
- (7) INCLUDES 8 TONS FOR COUNTY ROADS, BUSINESS ENTRANCES, DRIVEWAYS, TURN LANES, INTERSECTIONS, CROSS OVERS, HANDICAP PARKING AREA AND RAMPS.
- (8) TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER. USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
- (9) INCLUDES 603 TONS FOR COUNTY ROADS, BUSINESS ENTRANCES, DRIVEWAYS, TURN LANES, INTERSECTIONS, CROSS OVERS, HANDICAP PARKING AREA AND RAMPS.
- (10) INCLUDES 1552 TONS FOR COUNTY ROADS, BUSINESS ENTRANCES, DRIVEWAYS, TURN LANES, INTERSECTIONS, CROSS OVERS, HANDICAP PARKING AREA AND RAMPS. INCLUDES 130 TONS FOR SPOT REPAIR.
- (11) THIS ITEM IS FOR 3 STORM DRAIN MANHOLES
- (12) ITEM INCLUDES ADJUSTMENT OF 8 AREA DRAINS.
- (13) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (14) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
- (15) SEE SHEETS 2G11 AND 2G12 FOR DETAILS.
- (16) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (17) ITEM TO BE USED AS PERMANENT MARKING ONLY
- (18) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (19) TO BE USED AT L.M. 1.94 WESTBOUND IN FRONT OF ST. EDWARD CHURCH.
- (20) ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.
- (21) ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY. INCLUDES 6" DOTTED MARKINGS.
- (22) TO BE USED FOR CONCRETE SURFACES.
- (23) INCLUDES 3 EXIT ONLY ARROWS.
- (24) FOR PERMANENT EDGE LINE MARKING ON CONCRETE SURFACES.
- (25) FOR PERMANENT LANE LINE MARKING ON CONCRETE SURFACES. ITEM COST INCLUDES 2 LF WHITE MARKING AND 2 LF BLACK MARKING.
- (26) FOR PERMANENT LANE LINE MARKING ON CONCRETE SURFACES. ITEM COST INCLUDES 10 LF WHITE MARKING AND 10 LF BLACK MARKING.
- (27) SEE SIGNALIZATION TABULATION ON SHEET 2F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2-1

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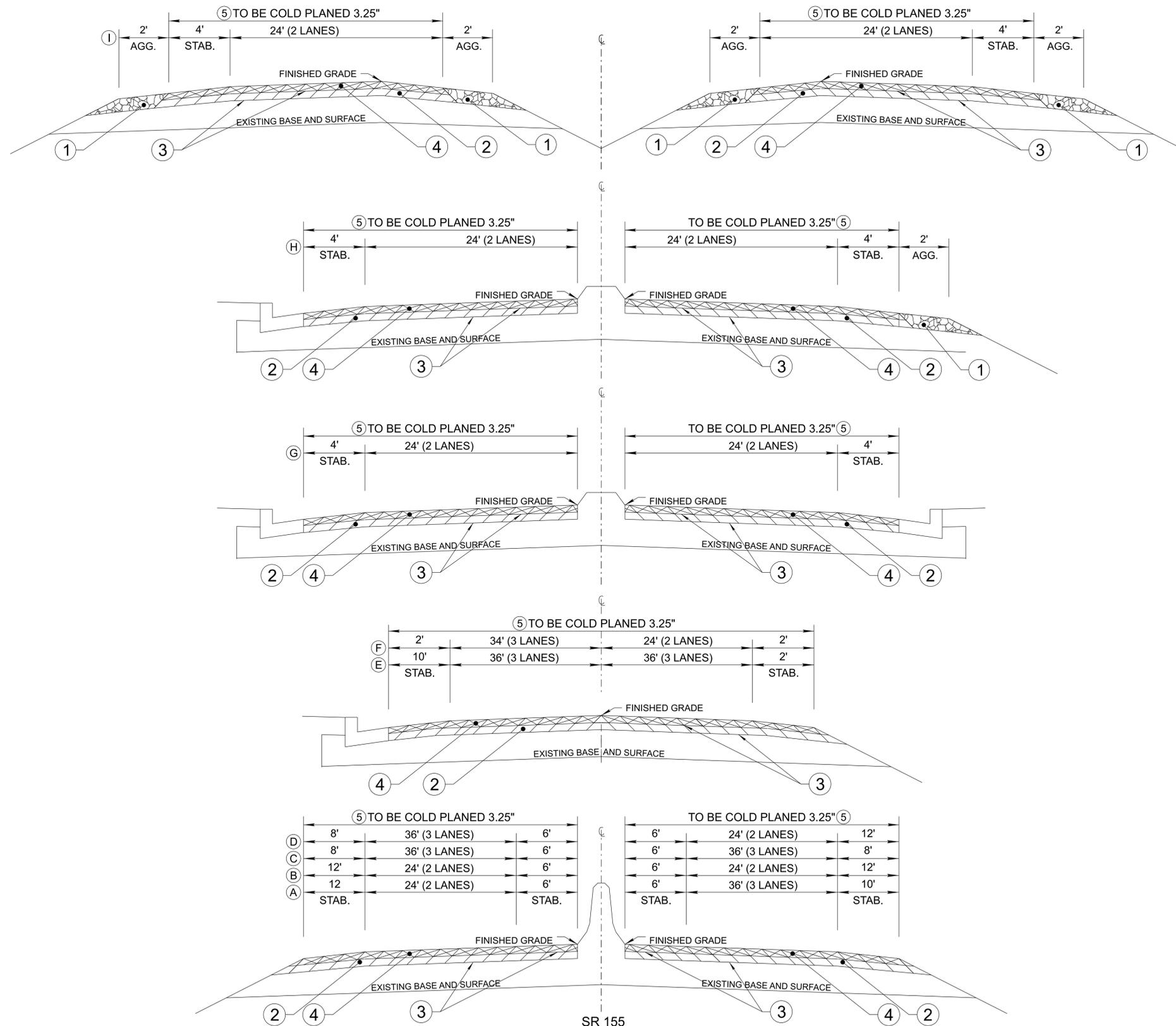


12/3/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2B



SR 155

TYPICAL SECTION

- (A) FROM: L.M. 0.00 TO L.M. 0.04 (CONCRETE) RESTRIPE EXISTING MARKINGS ONLY
- (B) FROM: L.M. 0.04 TO L.M. 0.11
- (C) FROM: L.M. 0.11 TO L.M. 0.21 (CONCRETE) RESTRIPE EXISTING MARKINGS ONLY
- (D) FROM: L.M. 0.21 TO L.M. 0.26
- (E) FROM: L.M. 0.26 TO L.M. 0.34
- (F) FROM: L.M. 0.34 TO L.M. 0.40
- (G) FROM: L.M. 0.40 TO L.M. 0.49
- (H) FROM: L.M. 0.49 TO L.M. 0.64
- (I) FROM: L.M. 0.64 TO L.M. 1.01

SEE SHEET 2B2 FOR PAVEMENT SCHEDULE

SEE SHEET 2F FOR BRIDGE RECOMMENDATIONS

SEALED BY



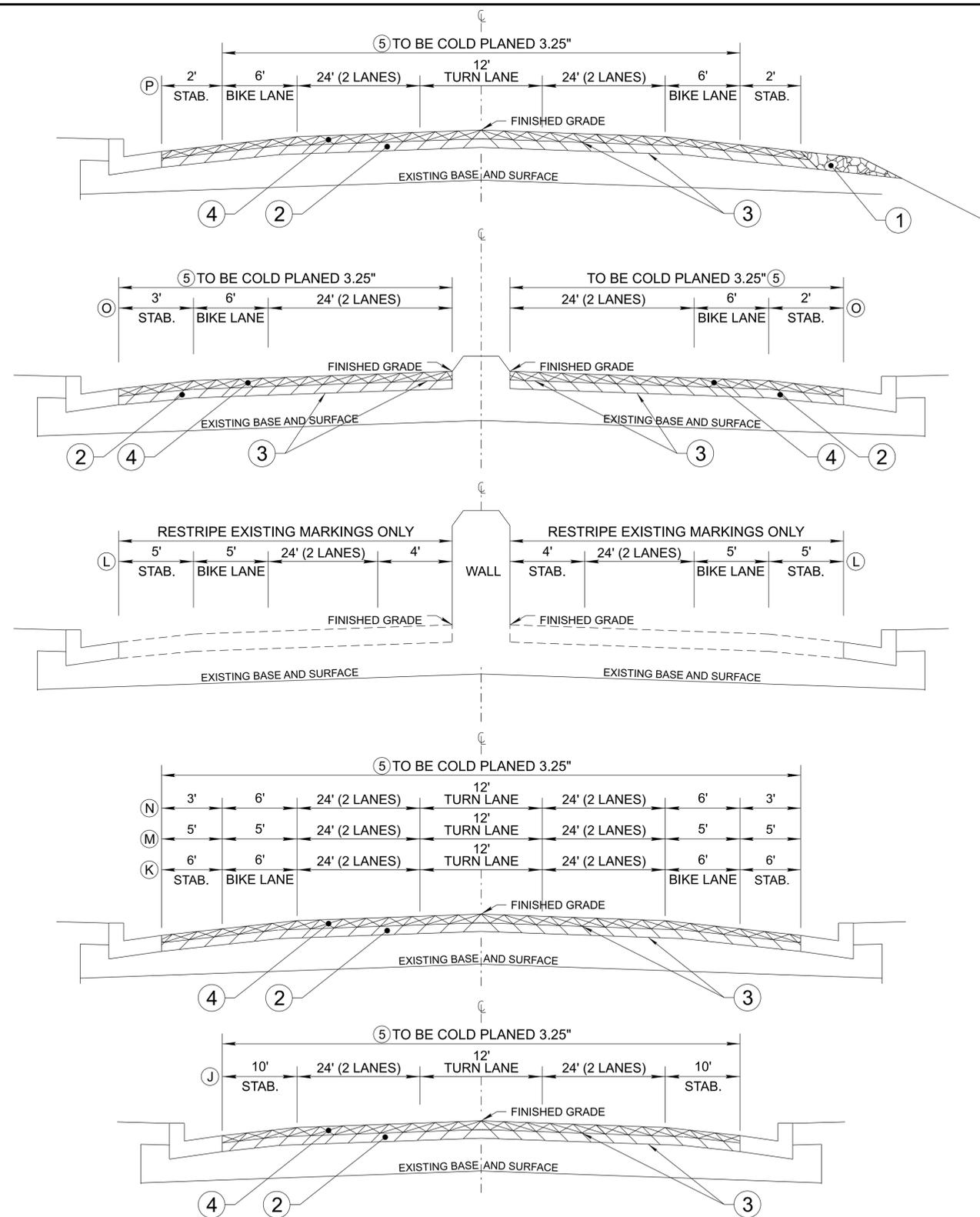
ANTHONY L. WASHINGTON
REGISTERED ENGINEER
AGRICULTURE
11111
STATE OF TENNESSEE
12/3/2025

NOT TO SCALE

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

TYPICAL
SECTIONS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2B1



SR 155

TYPICAL SECTION

- (J) FROM: L.M. 1.01 TO L.M. 1.10
- (K) FROM: L.M. 1.10 TO L.M. 1.26
- (L) FROM: L.M. 1.26 TO L.M. 1.32
- (M) FROM: L.M. 1.32 TO L.M. 1.36
- (N) FROM: L.M. 1.36 TO L.M. 1.88
- (O) FROM: L.M. 1.88 TO L.M. 1.97
- (P) FROM: L.M. 1.97 TO L.M. 2.42

SEE SHEET 2B2 FOR PAVEMENT SCHEDULE

SEALED BY



ANTHONY L. WASHINGTON
REGISTERED ENGINEER
AGRICULTURE
COMMERCIAL
19749
STATE OF TENNESSEE

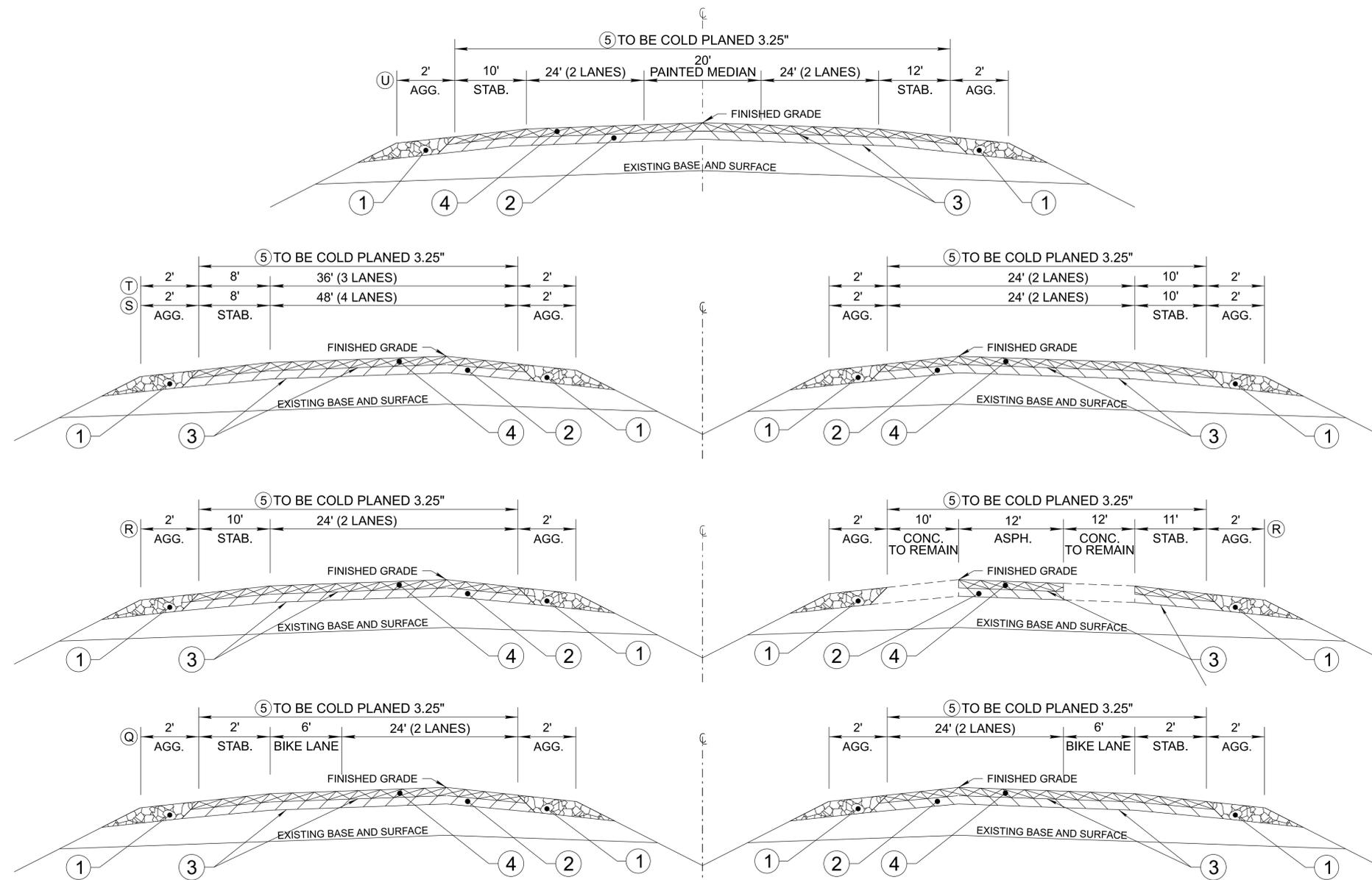
12/3/2025

NOT TO SCALE

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

TYPICAL
SECTIONS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2B2



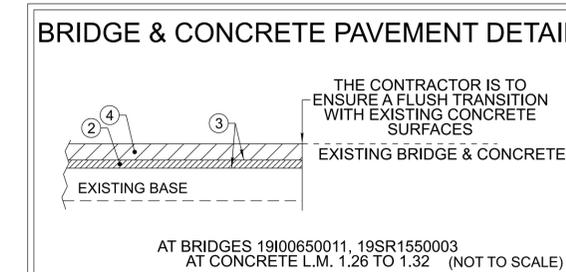
SR 155

TYPICAL SECTION

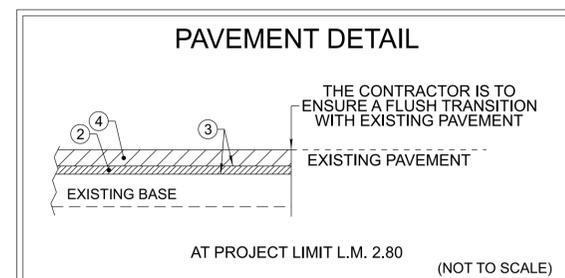
- Q FROM: L.M. 2.42 TO L.M. 2.50
- R FROM: L.M. 2.50 TO L.M. 2.59
- S FROM: L.M. 2.59 TO L.M. 2.68
- T FROM: L.M. 2.68 TO L.M. 2.76
- U FROM: L.M. 2.76 TO L.M. 2.80

PROPOSED PAVEMENT SCHEDULE

①	MINERAL AGGREGATE @ 2.00"± THICK FOR SHOULDERS
ITEM 303-01	MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"
②	BITUMINOUS COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.)
ITEM 307-02.08	ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING "B-M2"
③	TACK COAT
ITEM 403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
④	ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.)
ITEM 411-02.10	ACS MIX (PG70-22) GRADING "D"
⑤	COLD PLANING @ 3.25"± THICK (APPROX. 341.25 LBS./S.Y.)
ITEM 415-01.01	COLD PLANING BITUMINOUS PAVEMENT



AT BRIDGES 19I00650011, 19SR1550003
AT CONCRETE L.M. 1.26 TO 1.32 (NOT TO SCALE)



AT PROJECT LIMIT L.M. 2.80 (NOT TO SCALE)

SEALED BY

12/3/2025

NOT TO SCALE

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TYPICAL
SECTIONS**

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (7) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (8) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (5) AFTER THE SIGN LOCATIONS HAVE BEEN STAKED, BUT PRIOR TO ORDERING ANY MATERIAL FOR THE SUPPORTS, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE REGIONAL OPERATIONS OFFICE.
- (7) ALL SIGNS MARKED "TO BE REMOVED" ARE TO BE REMOVED BY THE CONTRACTOR AND PAID FOR UNDER ITEM NO. 713-15 AND BECOME THE PROPERTY OF THE CONTRACTOR.
- (9) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY SILK SCREENING PROCESS, EXCEPT THAT CUT-OUT DIRECT APPLIED COPY SHALL BE USED ON ALL FLAT SHEET SIGNS WITH A GREEN BACKGROUND, OR BROWN BACKGROUND.
- (10) THE LENGTHS OF ALL SIGN SUPPORTS SHOWN ON THE SIGN SCHEDULE ARE APPROXIMATE AND ARE FOR ESTIMATING PURPOSES ONLY. THE CONTRACTOR SHALL VERIFY ALL SUPPORT LENGTHS AT THE SITE PRIOR TO ERECTION.
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.
- (13) AFTER THE PERMANENT SIGN LOCATIONS HAVE BEEN STAKED, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE CONSTRUCTION FIELD OFFICE. PAYMENT FOR LOCATION AND STAKING SHOULD BE INCLUDED IN THE BID PRICE FOR OTHER ITEMS OF CONSTRUCTION. ANY RELOCATION REQUIRED, DUE TO THE SIGN NOT BEING INSTALLED IN THE CORRECT LOCATION, WILL BE DONE AT THE CONTRACTOR'S EXPENSE.

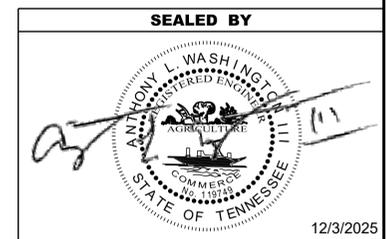
SIGNALIZATION

- (11) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- (12) THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.
- (14) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2C



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2D

SPECIAL NOTES

PAVEMENT

RESURFACING

- (2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - A. BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - B. ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - C. ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
- (3) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (4) WHEN A PERFORMANCE GRADE (PG) ASPHALT MIX WITH PROPERTIES GREATER THAN THAT OF PG64-22 IS CALLED FOR ON RESURFACING PLANS AND IS THE ONLY ASPHALT GRADE ON THE PROJECT, THE CONTRACTOR HAS THE OPTION OF USING EITHER THE ASPHALT GRADE SHOWN IN THE PLANS OR AN ASPHALT GRADE EQUAL TO OR BETTER THAN PG64-22 FOR DRIVEWAYS AND BUSINESS ENTRANCES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE MATERIAL WILL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THE ASPHALT OR ASPHALT MIX. THE MATERIAL TICKETS SHALL BE MARKED "FOR DRIVEWAYS AND BUSINESS ENTRANCES ONLY" AT THE POINT OF MANUFACTURE.
- (5) THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (6) THE OUTSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE
- (8) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.
- (10) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

STORM DRAIN STRUCTURES

- (1) TAPER AROUND ALL CATCH BASINS, AREA DRAINS, AND MANHOLES AS DIRECTED BY THE ENGINEER.
- (2) THE CONTRACTOR SHALL PROPERLY ADJUST ALL CATCH BASINS, AREA DRAINS, AND MANHOLES TO THE FINISHED GRADE OF THE PAVEMENT. EXTENSION RINGS SHALL BE ADDED/REMOVED FROM EXISTING CATCH BASINS, AREA DRAINS, AND MANHOLES TO RAISE/LOWER THE GRATE OR LID TO FINAL PAVEMENT ELEVATION. ADJUSTMENTS ARE TO BE MADE PRIOR TO THE LAYING OF SURFACE MIX ITEM 411-02.10. PAYMENT FOR MANHOLE AND CATCH BASIN ADJUSTMENTS WILL BE MADE AT THE UNIT PRICE BID FOR ITEMS 611-01.20 AND 611-09.01 RESPECTIVELY.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

MISCELLANEOUS

- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

JOINT SEALANTS

- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN ALL PAVEMENT LAYERS EXCEPT OGFC SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (3) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (4) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

CSXT SAFETY NOTES

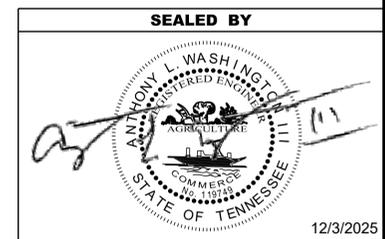
- (1) COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (2) ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE DONE ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (3) ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (4) CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY.
- (5) ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (6) ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (7) AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (8) THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (9) THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT'S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS.
- (10) PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD'S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (11) ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.

- (12) NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (13) ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORK DAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (14) ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (15) "ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
- (16) ALL SOILS EXCAVATED WITHIN CSXT'S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT'S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.

THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE
CROUCH ENGINEERING, INC.

5115 MARYLAND WAY, SUITE 225
BRENTWOOD, TN 37027
ATTN: MR. SCOTT VICK, P.E. PROJECT DIRECTOR
PHONE: (615) 791-0630
EMAIL: SVICK@CROUCHENGINEERING.COM



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2E

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE PROJECT MANAGER SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL

- (20) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES MILL, B-M2, 411D, STRIPING, TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

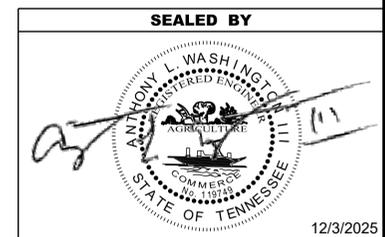
SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

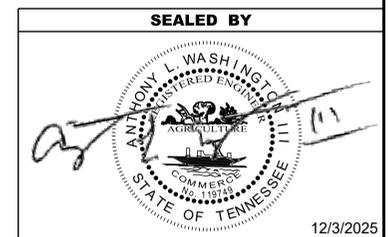
ENVIRONMENTAL NOTES

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2E1



**STATE OF TENNESSEE
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**ENVIRONMENTAL
NOTES**

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)

M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x	W			
G20-1	ROAD WORK NEXT 3 MILES	48"	x	24"	8	2	16
G20-2	END ROAD WORK	48"	x	24"	8	30	240
R3-7L	LEFT LANE MUST TURN LEFT	48"	x	48"	16	2	32
W4-2L	LEFT LANE ENDS SYMBOL	48"	x	48"	16	4	64
W4-2R	RIGHT LANE ENDS SYMBOL	48"	x	48"	16	4	64
W8-11	UNEVEN LANES	48"	x	48"	16	14	224
W8-15	GROOVED PAVEMENT	48"	x	48"	16	14	224
W8-15P	MOTORCYCLE PLAQUE	30"	x	24"	5	14	70
W13-1P	ADVISORY SPEED PLAQUE	24"	x	24"	4	14	56
W20-1	ROAD WORK AHEAD	48"	x	48"	16	28	448
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	2	32
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	2	32
W20-1	ROAD WORK 1000 FEET	48"	x	48"	16	2	32
W20-1	ROAD WORK 500 FEET	48"	x	48"	16	2	32
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16	4	64
W20-5L	LEFT LANE CLOSED 1500 FEET	48"	x	48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1500 FEET	48"	x	48"	16	4	64
W21-2	FRESH OIL	48"	x	48"	16	4	64
W21-5	SHOULDER WORK	48"	x	48"	16	4	64
TOTAL						1950	

SIGNALIZATION TABULATION (RESURFACING)

INTERSECTION	SAW SLOT (L.F.)	LOOP WIRE (L.F.)
SR-155 @ POWELL AVE & BRANSFORD AVE	802	1804
SR-155 @ SIDCO DR	802	1804
SR-155 @ EUGENIA AVE	802	1804
SR-155 @ NOLENSVILLE PK	802	1804
TOTALS	3208	7216

UTILITY ADJUSTMENTS (RESURFACING)

TYPE	OWNER	NUMBER
SEWER MANHOLE	METRO WATER	16
WATER VALVES	METRO WATER	33
GAS VALVES	PIEDMONT GAS	9

TYPE	YEAR	PROJECT NO.	SHEET NO.
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BRIDGE DECK RECOMMENDATIONS (RESURFACING)

BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
19I00650011	0.110	I-65 & CSX RR UNDERPASS	509.72'	PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
19SR1550003	0.300	BRANCH	30'	PAVE WITH PLANS MIX/TREATMENT TYPE
19SR1550005	1.260	CSXT OVERPASS		

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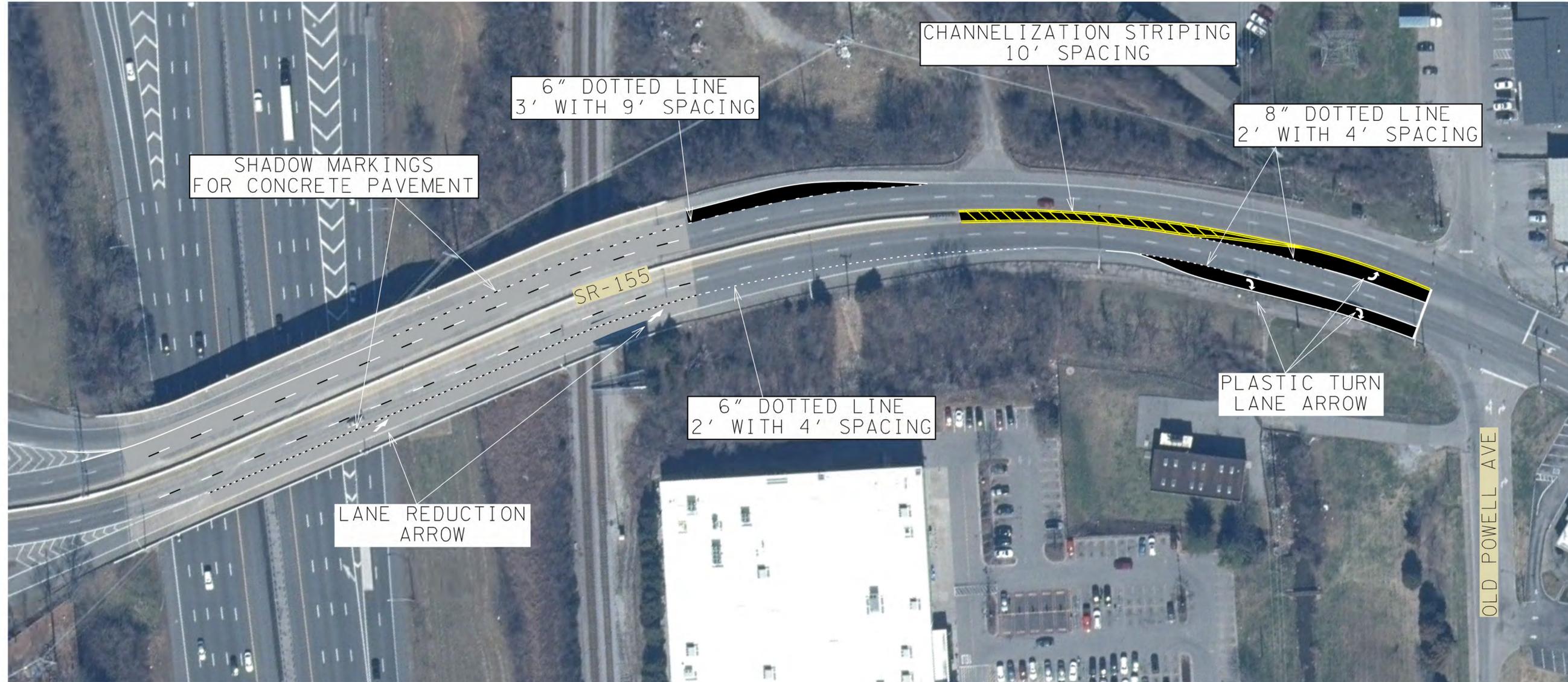
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12/3/2025

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TABULATED
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G



STATE ROUTE 155
0.13 - 0.34 LM
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-3, T-M-4, T-M-5, T-M-6

FOR REFERENCE ONLY
PROVIDED BY REGION 3 TRAFFIC



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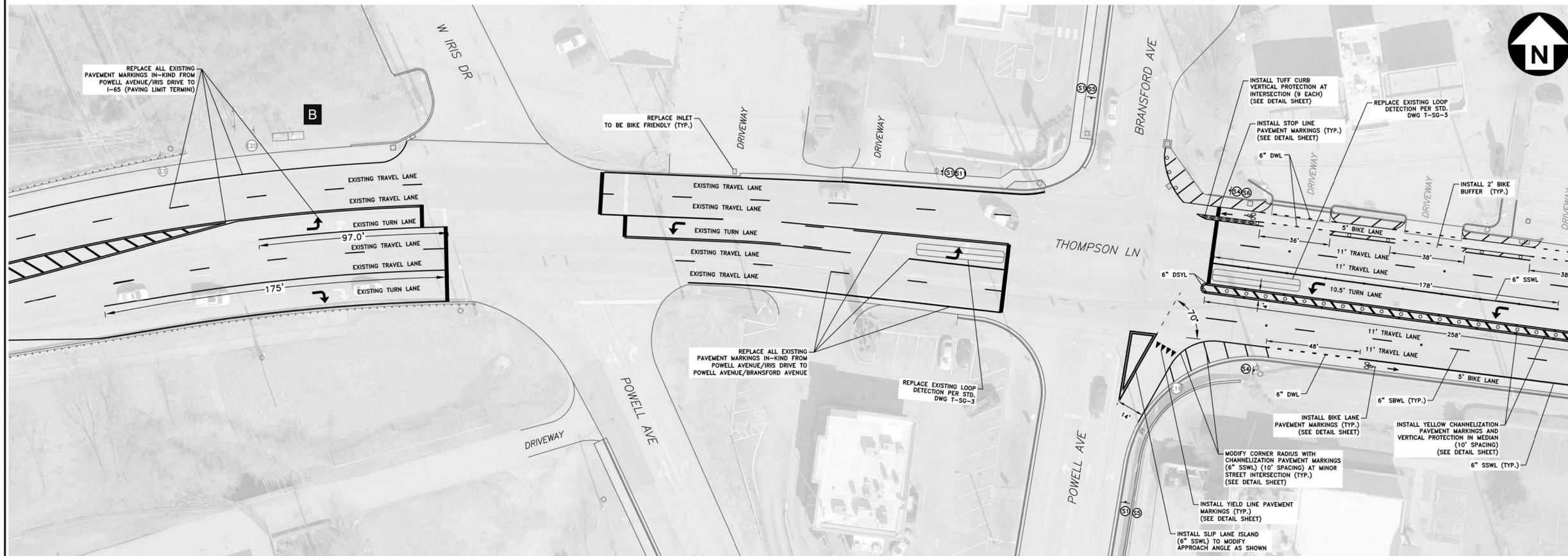
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

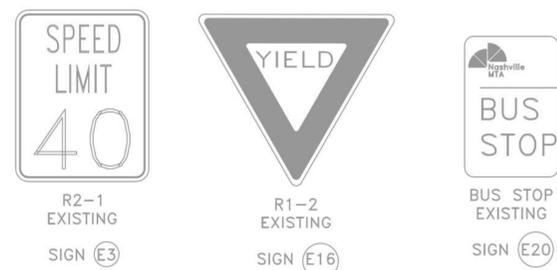
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G1

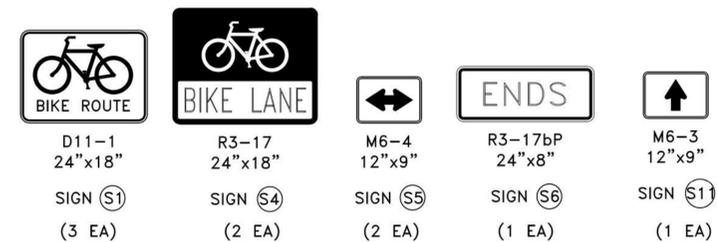


MATCH LINE SHEET 2G2

TRAFFIC SIGNS
(EXISTING TO REMAIN)



TRAFFIC SIGNS
(NEW)



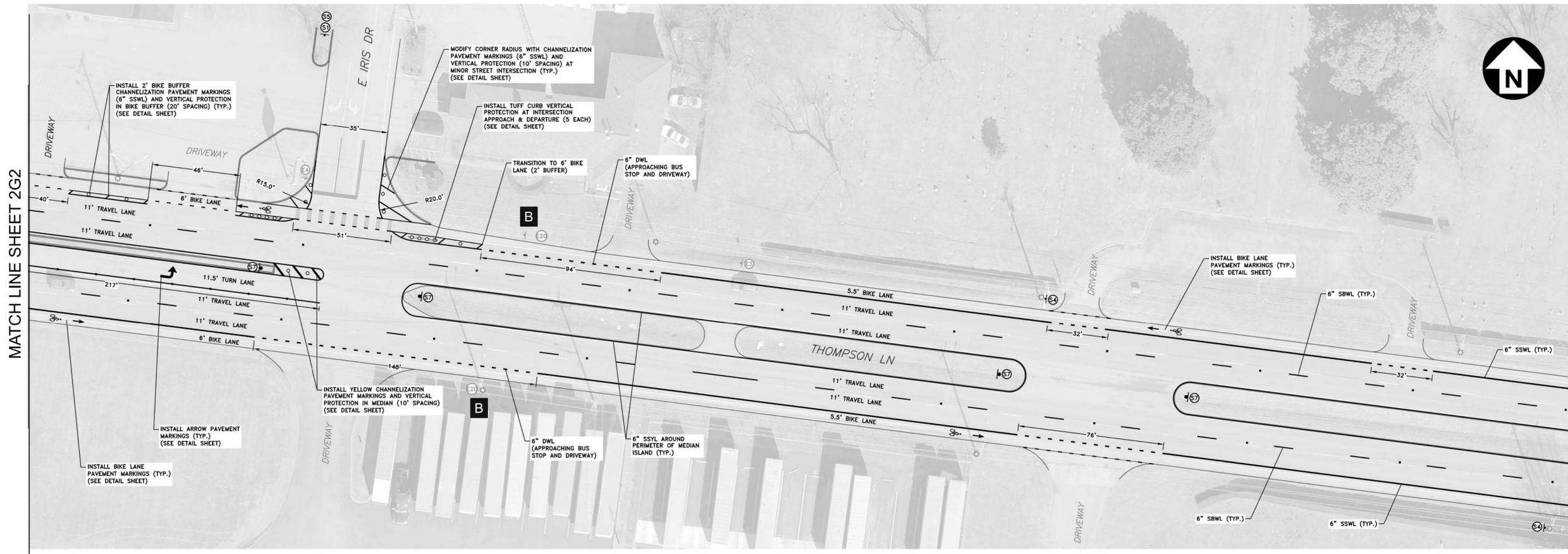
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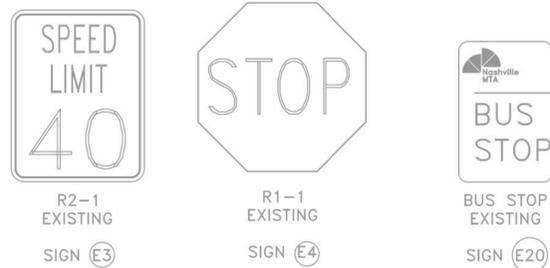
STATE OF TENNESSEE
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PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

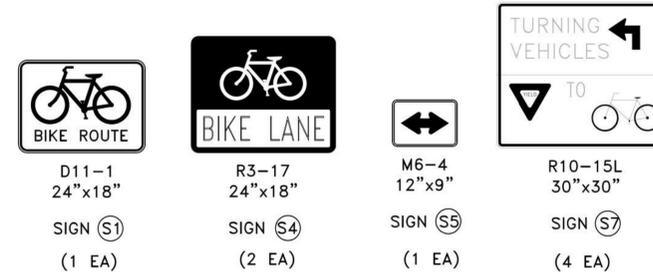
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G3



TRAFFIC SIGNS
(EXISTING TO REMAIN)



TRAFFIC SIGNS
(NEW)



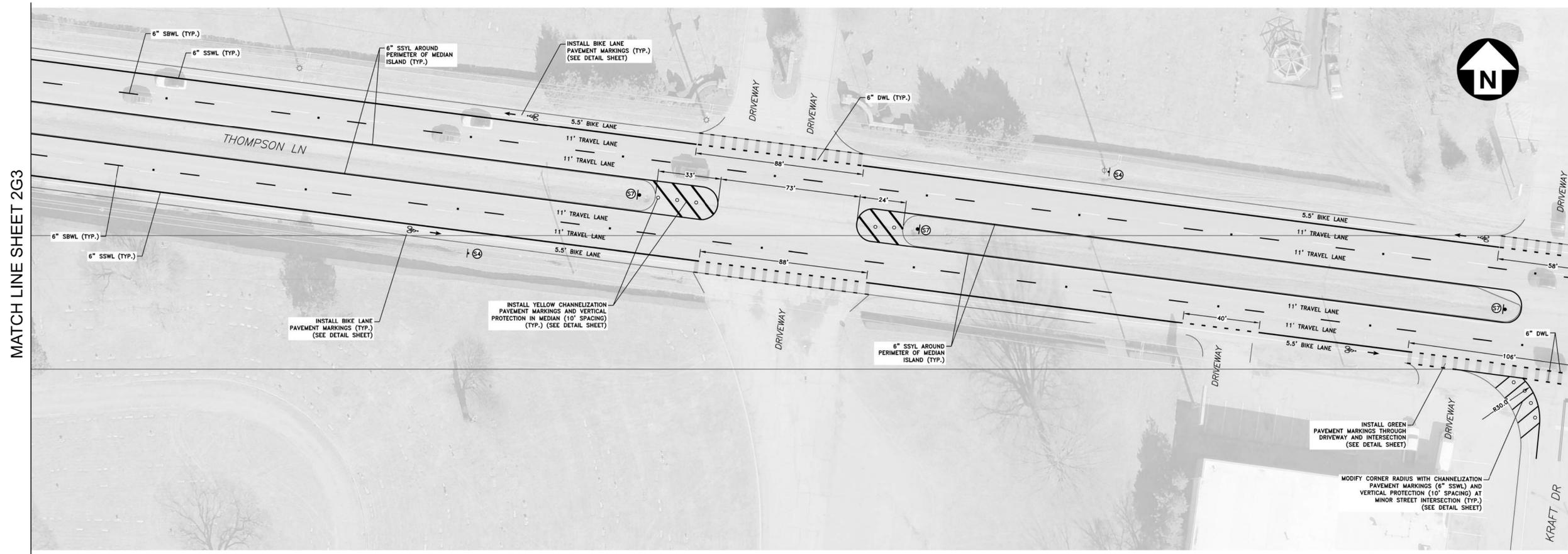
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STATE OF TENNESSEE
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PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G4



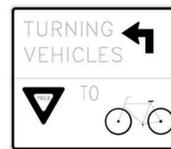
MATCH LINE SHEET 2G3

MATCH LINE SHEET 2G5

TRAFFIC SIGNS
(NEW)



R3-17
24"x18"
SIGN (S4)
(2 EA)



R10-15L
30"x30"
SIGN (S7)
(3 EA)

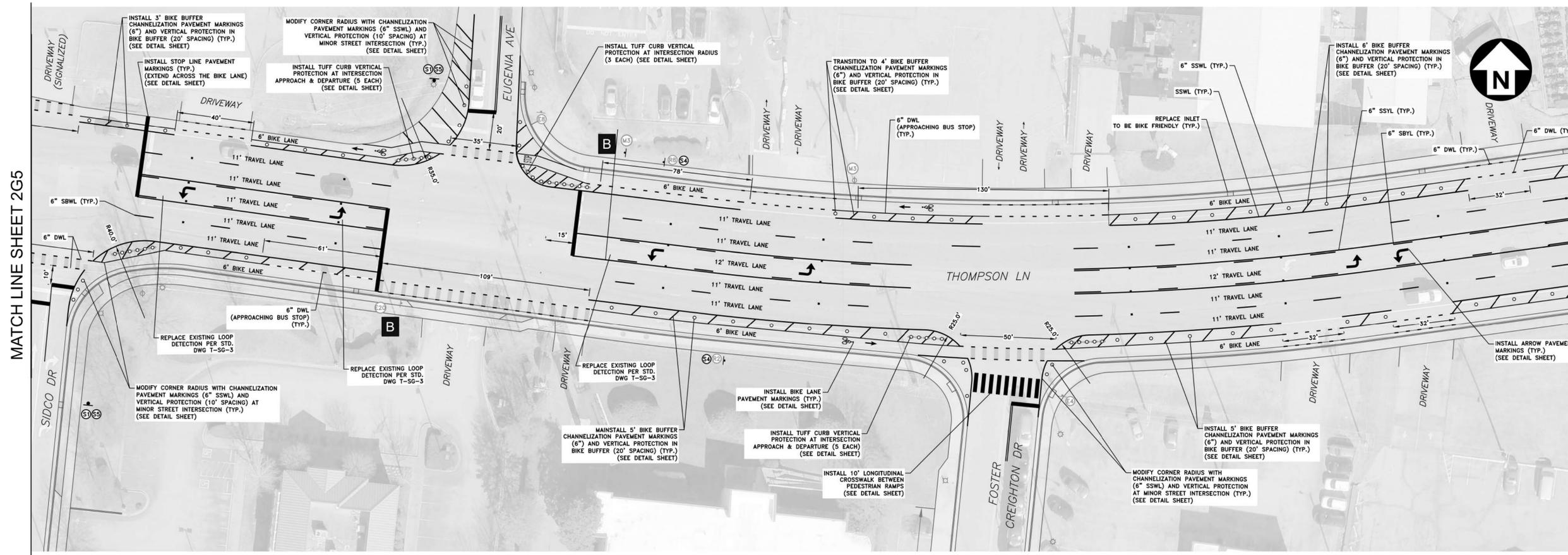
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NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

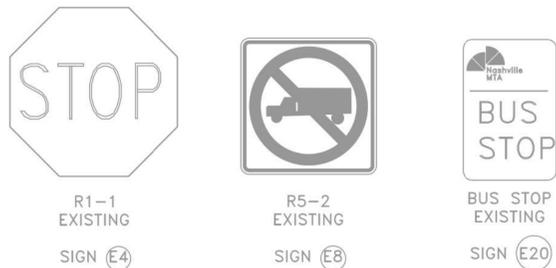
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G6



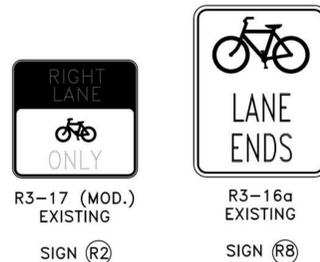
MATCH LINE SHEET 2G5

MATCH LINE SHEET 2G7

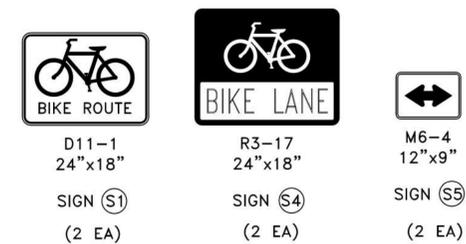
**TRAFFIC SIGNS
(EXISTING TO REMAIN)**



**TRAFFIC SIGNS
(TO BE REMOVED)**



**TRAFFIC SIGNS
(NEW)**



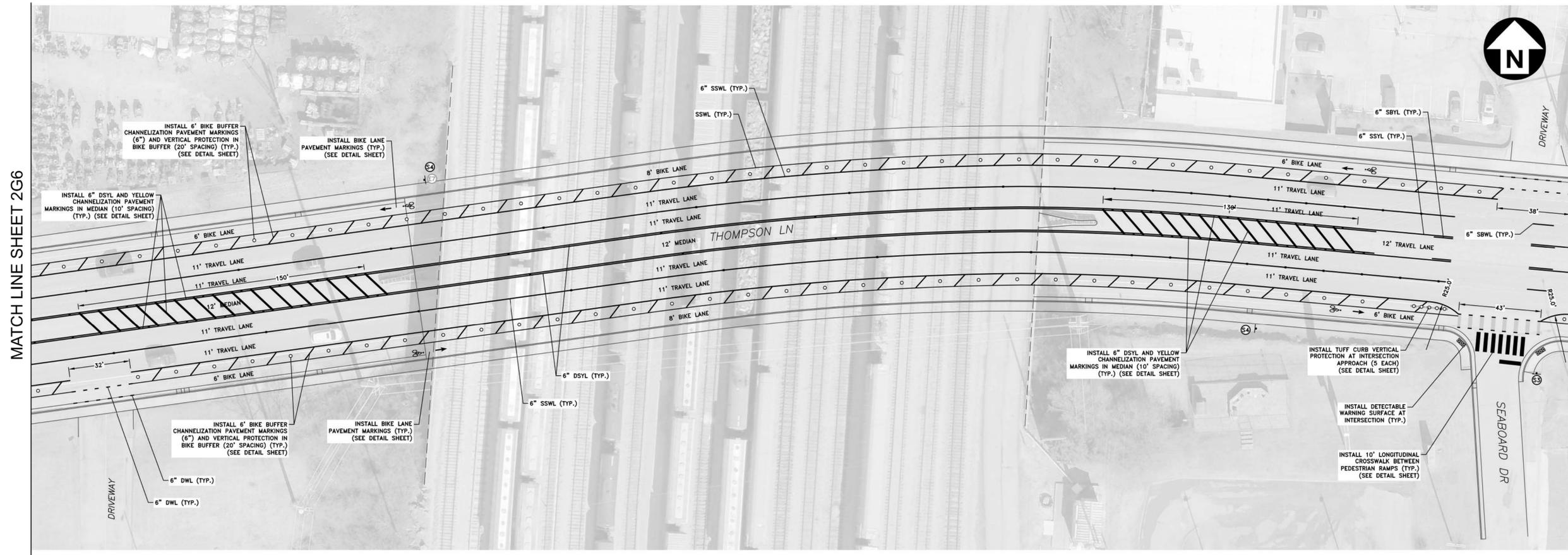
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NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G7



MATCH LINE SHEET 2G6

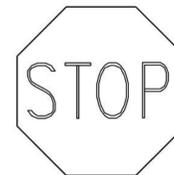
MATCH LINE SHEET 2G8

TRAFFIC SIGNS
(EXISTING TO REMAIN)



R3-17 (MOD.)
EXISTING
SIGN (E7)

TRAFFIC SIGNS
(NEW)



R1-1
36" x 36"
SIGN (S3)
(1 EA)



R3-17
24" x 18"
SIGN (S4)
(2 EA)

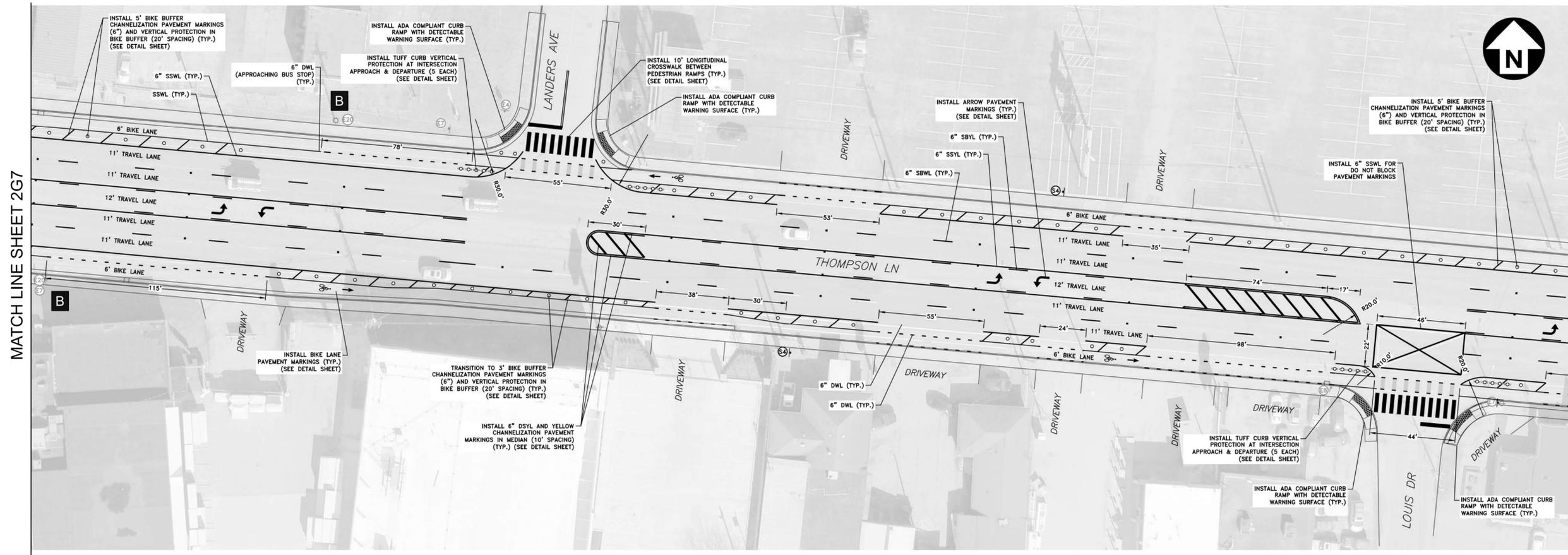
SEALED BY

NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G8



MATCH LINE SHEET 2G7

MATCH LINE SHEET 2G9

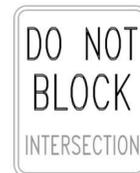
TRAFFIC SIGNS
(EXISTING TO REMAIN)



R1-1
EXISTING
SIGN E4



R3-17 (MOD.)
EXISTING
SIGN E7



R10-7
EXISTING
SIGN E8



BUS STOP
EXISTING
SIGN E20

TRAFFIC SIGNS
(NEW)



R3-17
24"x18"
SIGN S4
(2 EA)

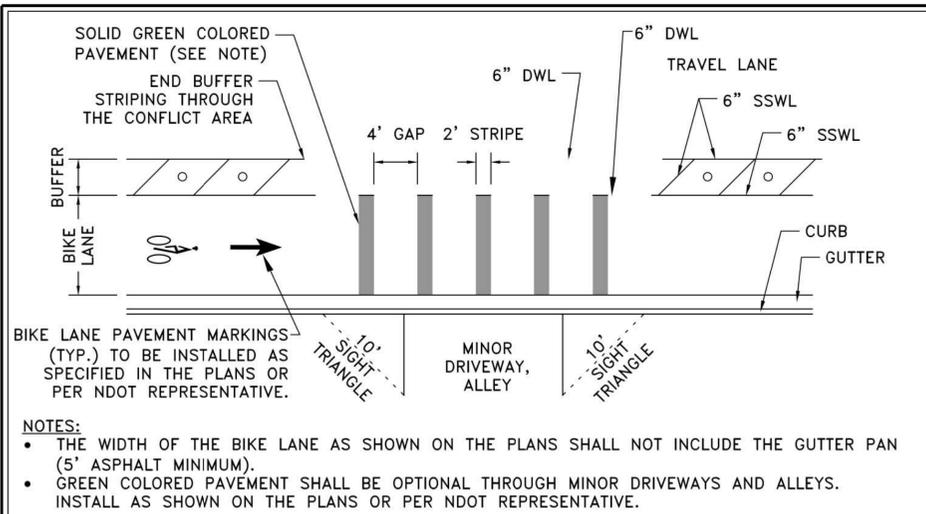
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NOT TO SCALE

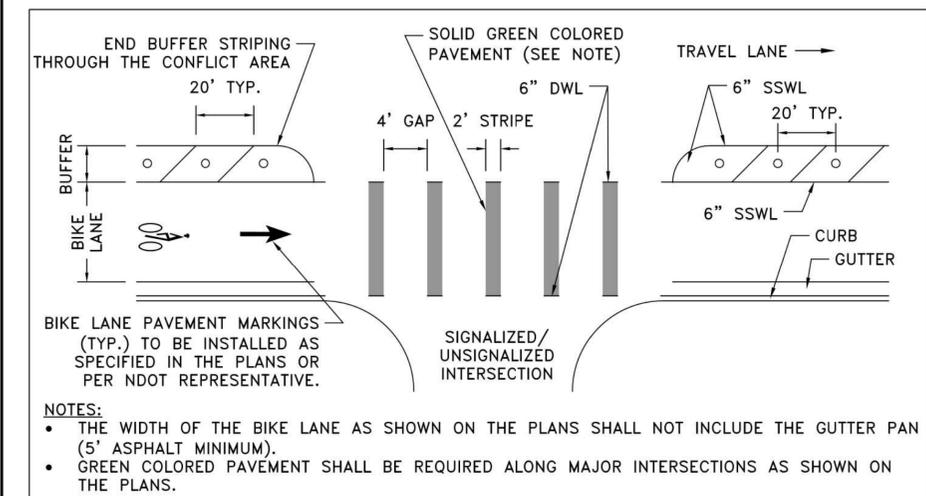
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

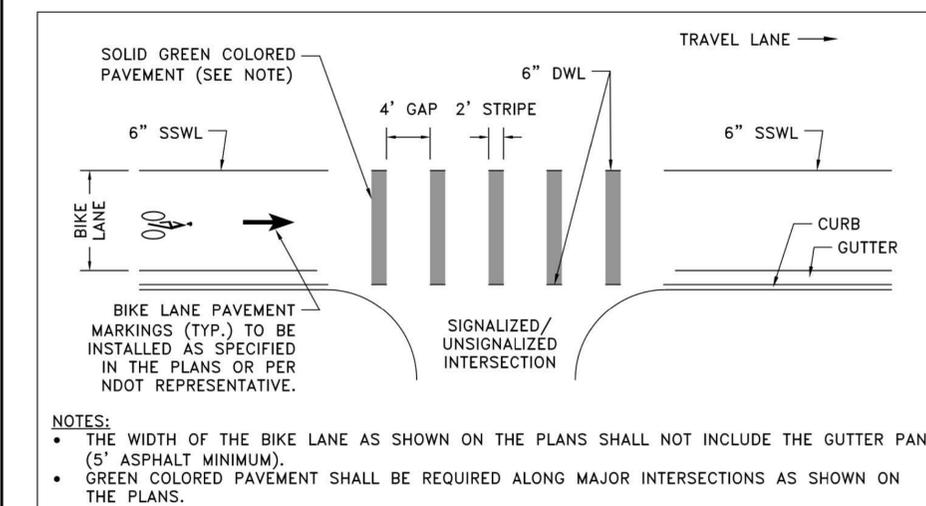
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G10



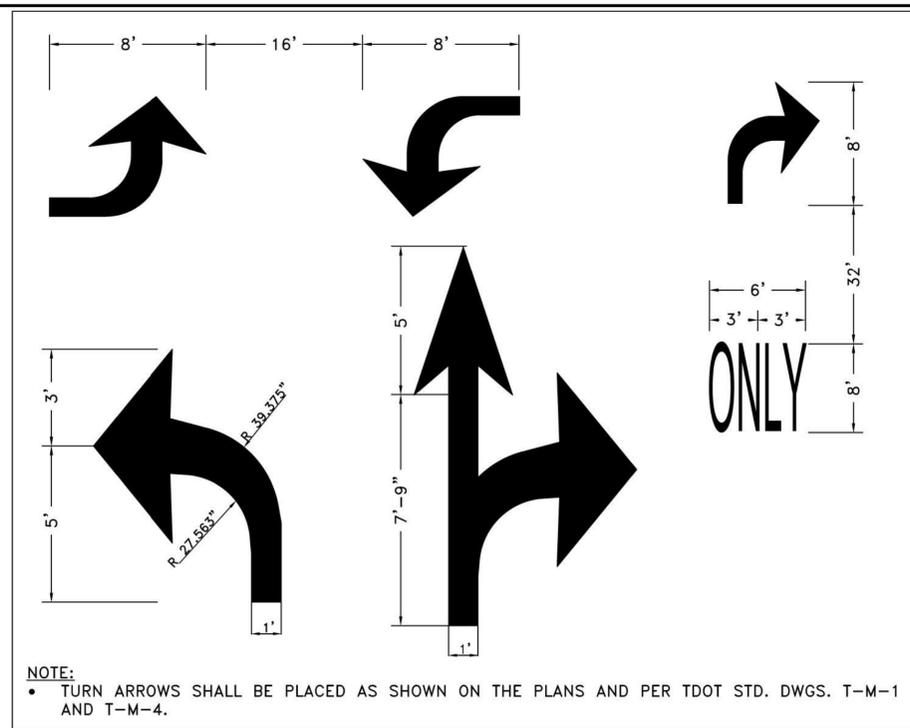
BUFFERED BIKE LANE SITE TRIANGLE & BIKE CONFLICT LINE (BLC) DETAIL AT DRIVEWAYS
N.T.S.



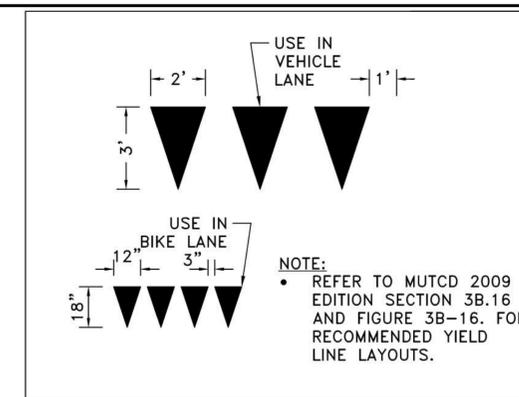
BUFFERED BIKE LANE CONFLICT LINE (BLC) AT INTERSECTIONS
N.T.S.



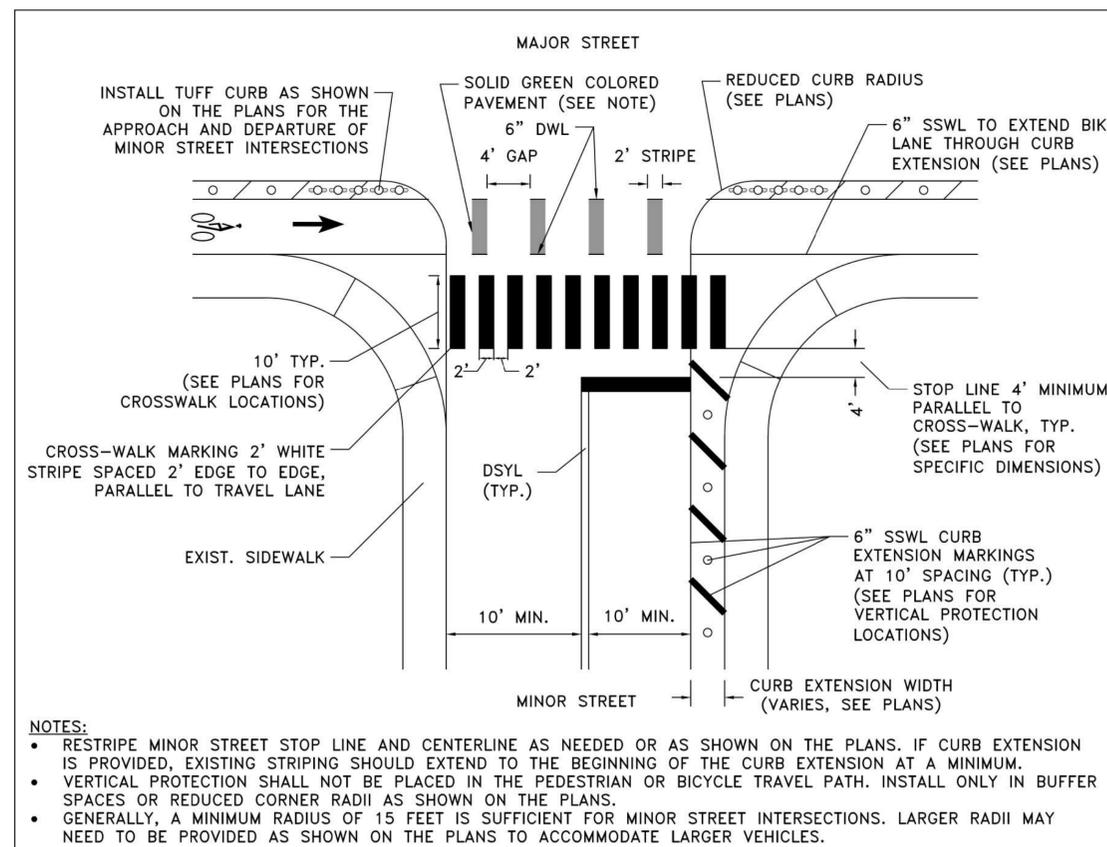
BIKE LANE CONFLICT LINE (BLC) AT INTERSECTIONS
N.T.S.



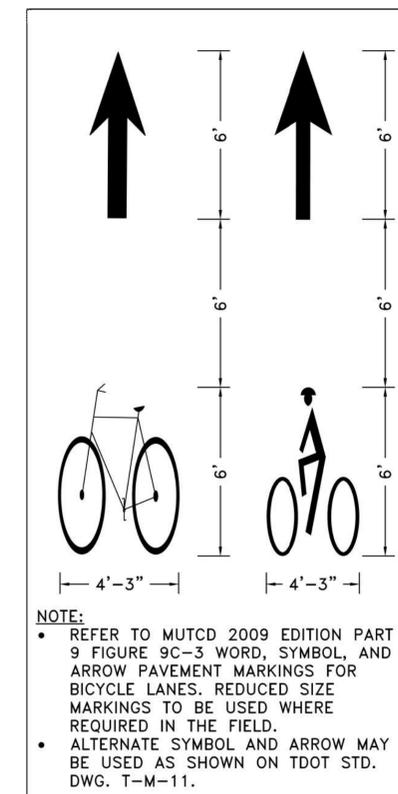
STANDARD ARROW PAVEMENT MARKINGS
N.T.S.



YIELD LINE DETAILS
N.T.S.



TYPICAL CURB EXTENSION FOR MINOR STREET INTERSECTION (STOP LINE PLACEMENT AND LONGITUDINAL CROSSWALK MARKINGS)
N.T.S.



BIKE LANE PAVEMENT MARKING DETAIL
N.T.S.

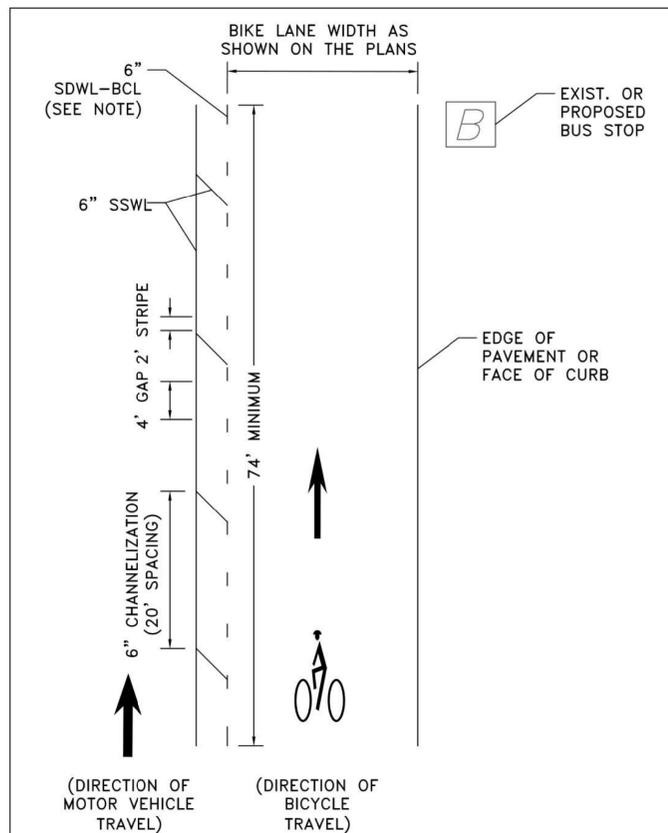
SEALED BY

NOT TO SCALE

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

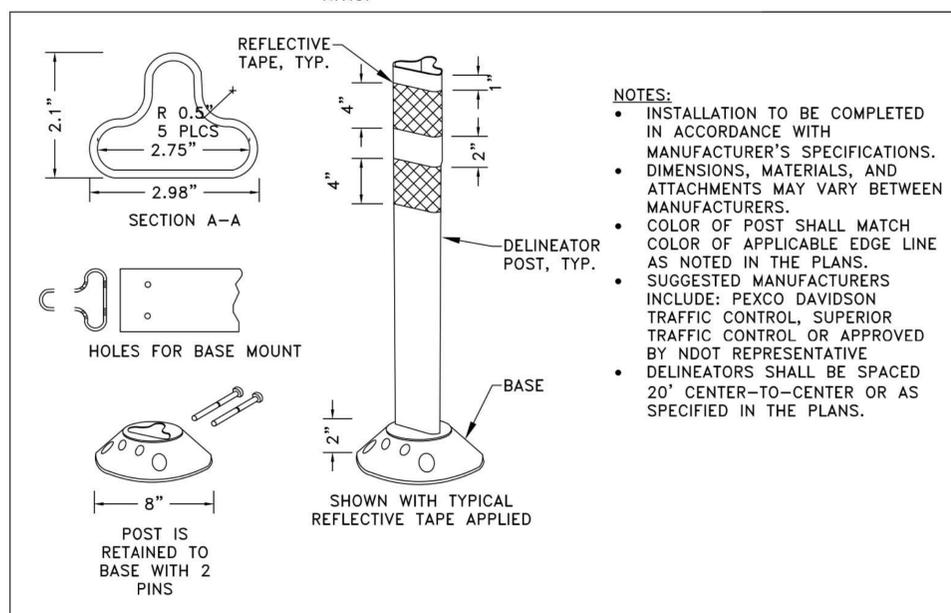
PAVEMENT MARKING IMPROVEMENTS AND SIGNING PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G11



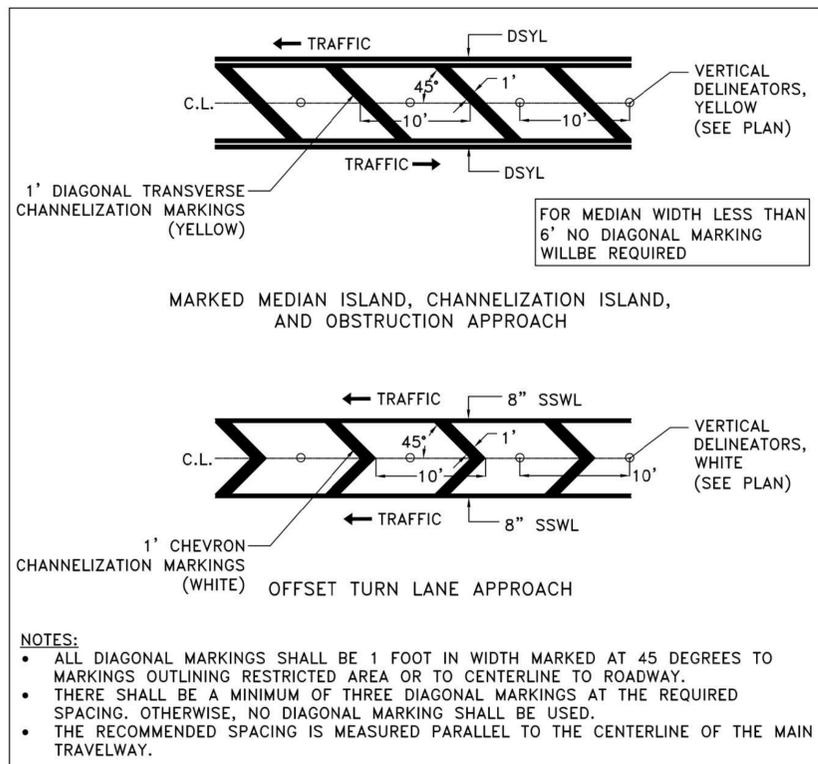
- NOTES:**
- IF NOT BUFFER IS PROVIDED, THE DOTTED WHITE LINE SHALL BE THE SAME WIDTH AS THE EDGELINE THAT DELINEATES CONTINUATION OF THE BIKE LANE THROUGH THE BUS STOP.
 - VERTICAL PROTECTION SHALL NOT BE PLACED WITHIN THE FULL WIDTH OF THE BUS STOP AREA.
 - CONTRACTOR SHALL COORDINATE WITH NDOT AND WEGO TO REMOVE, RELOCATE, OR INSTALL BUS STOP SIGNS AS NEEDED.

BIKE LANE AND BUS STOP PAVEMENT MARKING DETAIL
N.T.S.



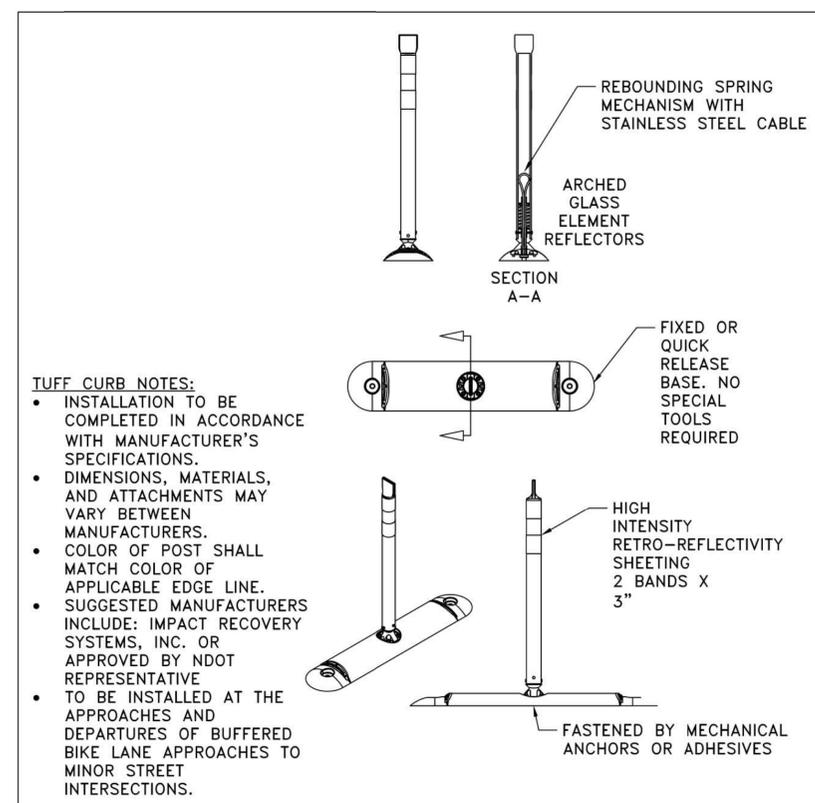
- NOTES:**
- INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 - DIMENSIONS, MATERIALS, AND ATTACHMENTS MAY VARY BETWEEN MANUFACTURERS.
 - COLOR OF POST SHALL MATCH COLOR OF APPLICABLE EDGE LINE AS NOTED IN THE PLANS.
 - SUGGESTED MANUFACTURERS INCLUDE: PEXCO DAVIDSON TRAFFIC CONTROL, SUPERIOR TRAFFIC CONTROL OR APPROVED BY NDOT REPRESENTATIVE
 - DELINEATORS SHALL BE SPACED 20' CENTER-TO-CENTER OR AS SPECIFIED IN THE PLANS.

36" FLEXIBLE DELINEATOR POST DETAIL
N.T.S.



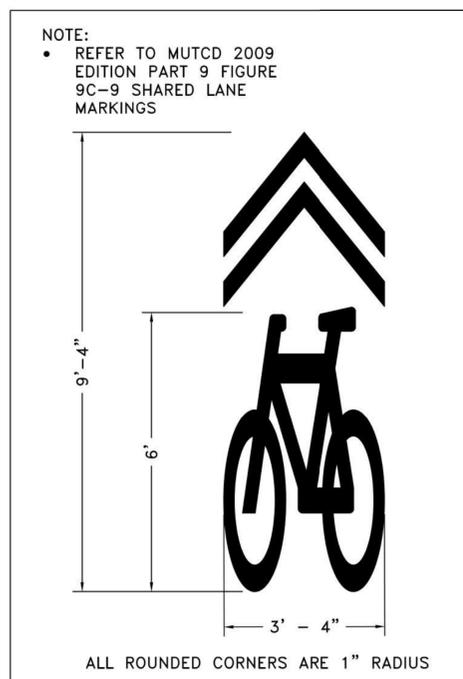
- NOTES:**
- ALL DIAGONAL MARKINGS SHALL BE 1 FOOT IN WIDTH MARKED AT 45 DEGREES TO MARKINGS OUTLINING RESTRICTED AREA OR TO CENTERLINE TO ROADWAY.
 - THERE SHALL BE A MINIMUM OF THREE DIAGONAL MARKINGS AT THE REQUIRED SPACING. OTHERWISE, NO DIAGONAL MARKING SHALL BE USED.
 - THE RECOMMENDED SPACING IS MEASURED PARALLEL TO THE CENTERLINE OF THE MAIN TRAVELWAY.

CHANNELIZATION MARKINGS
N.T.S.



- TUFF CURB NOTES:**
- INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 - DIMENSIONS, MATERIALS, AND ATTACHMENTS MAY VARY BETWEEN MANUFACTURERS.
 - COLOR OF POST SHALL MATCH COLOR OF APPLICABLE EDGE LINE.
 - SUGGESTED MANUFACTURERS INCLUDE: IMPACT RECOVERY SYSTEMS, INC. OR APPROVED BY NDOT REPRESENTATIVE
 - TO BE INSTALLED AT THE APPROACHES AND DEPARTURES OF BUFFERED BIKE LANE APPROACHES TO MINOR STREET INTERSECTIONS.

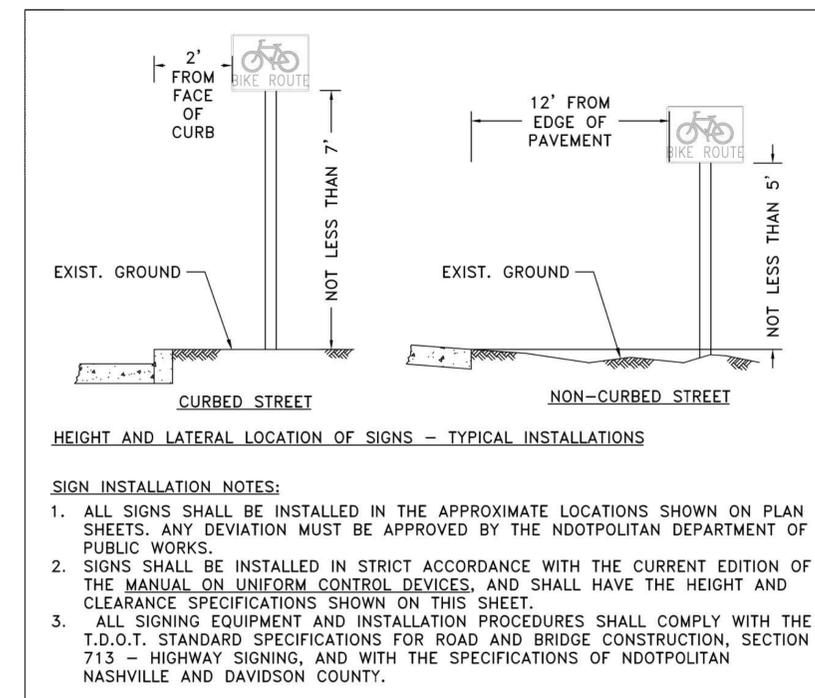
TUFF CURB XLP WITH TUBULAR MARKER DETAIL
N.T.S.



- NOTE:**
- REFER TO MUTCD 2009 EDITION PART 9 FIGURE 9C-9 SHARED LANE MARKINGS

ALL ROUNDED CORNERS ARE 1" RADIUS

SHARROW DETAIL
N.T.S.



HEIGHT AND LATERAL LOCATION OF SIGNS - TYPICAL INSTALLATIONS

SIGN INSTALLATION NOTES:

1. ALL SIGNS SHALL BE INSTALLED IN THE APPROXIMATE LOCATIONS SHOWN ON PLAN SHEETS. ANY DEVIATION MUST BE APPROVED BY THE NDOTPOLITAN DEPARTMENT OF PUBLIC WORKS.
2. SIGNS SHALL BE INSTALLED IN STRICT ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM CONTROL DEVICES, AND SHALL HAVE THE HEIGHT AND CLEARANCE SPECIFICATIONS SHOWN ON THIS SHEET.
3. ALL SIGNING EQUIPMENT AND INSTALLATION PROCEDURES SHALL COMPLY WITH THE T.D.O.T. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 713 - HIGHWAY SIGNING, AND WITH THE SPECIFICATIONS OF NDOTPOLITAN NASHVILLE AND DAVIDSON COUNTY.

HEIGHT AND LATERAL LOCATION OF SIGNS - TYPICAL INSTALLATION
N.T.S.

SEALED BY

NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

ALL SIGNS SHOWN WITH DESIGNATIONS ARE TO BE FABRICATED AS DETAILED IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION)

THE FOLLOWING STANDARD DRAWINGS APPLY UNLESS OTHERWISE NOTED IN THE REMARKS: FLAT SHEET (T-S SERIES 10, 12, 16, 17, 19, 20); EXTRUDED PANEL (T-S SERIES 6, 9, 13, 14); WALL/BARRIER MOUNTED (T-S-21), MULTI-DIRECTIONAL BASE (T-S SERIES 23A, 23B, 23C); RAILROAD (T-S-16)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G12

SIGN NO	LEGEND	SHEET NO	SIZE			COPY					SHIELD	ARROW	SIGN FACE			STEEL DESIGN (BREAK-AWAY)					REMARKS	
			LENGTH	HEIGHT	RADIUS	BORDER WIDTH	CAPITAL	LOWER CASE	NUMERAL	SERIES			COPY	BACKGROUND	MATERIAL	SUPPORT TYPE	SUPPORT LENGTH	FOOTING	CONC. CU. YD.	REIN. STEEL LBS.		MINIMUM VERTICAL CLEARANCE
PROPOSED SIGNS																						
S1	D11-1	2G1-3, 2G6, 2G9	24"	18"	-	-	-	-	-	-	-	-	WHITE	GREEN	0.080" SHEET ALUMINUM	P3/P8	12'	TYPE 4	-	-	7' - 0"	
S2	M6-6	2G9	12"	9"	-	-	-	-	-	-	-	-	WHITE	GREEN	0.080" SHEET ALUMINUM	-	-	-	-	-	7' - 0"	MOUNTED BELOW SIGN S1
S3	R1-1	2G7	36"	36"	-	-	-	-	-	-	-	-	WHITE	RED	0.080" SHEET ALUMINUM	P3/P8	12'	TYPE 4	-	-	7' - 0"	
S4	R3-17	2G1-8	24"	18"	-	-	-	-	-	-	-	-	BLACK WHITE	BLACK WHITE	0.080" SHEET ALUMINUM	P3/P8	12'	TYPE 4	-	-	7' - 0"	
S5	M6-4	2G1-3, 2G6	12"	9"	-	-	-	-	-	-	-	-	WHITE	GREEN	0.080" SHEET ALUMINUM	-	-	-	-	-	7' - 0"	MOUNTED BELOW SIGN S1
S6	R3-17bP	2G1	24"	8"	-	-	-	-	-	-	-	-	BLACK	WHITE	0.080" SHEET ALUMINUM	-	-	-	-	-	7' - 0"	MOUNTED BELOW SIGN S4
S7	R10-15L	2G3-5	30"	30"	-	-	-	-	-	-	-	-	RED BLACK	WHITE YELLOW	0.080" SHEET ALUMINUM	P3/P8	-	-	-	-	7' - 0"	
S8	R1-2	2G9	48"	48"	-	-	-	-	-	-	-	-	RED	RED WHITE	0.080" SHEET ALUMINUM	P3/P8	-	-	-	-	7' - 0"	
S9	R10-15R	2G9	30"	30"	-	-	-	-	-	-	-	-	RED BLACK	WHITE YELLOW	0.080" SHEET ALUMINUM	P3/P8	-	-	-	-	7' - 0"	
S10	R1-5A (MOD)	2G9	36"	48"	-	-	-	-	-	-	-	-	RED BLACK	WHITE	0.080" SHEET ALUMINUM	P3/P8	-	-	-	-	7' - 0"	
S11	2G1	2G1	12"	9"	-	-	-	-	-	-	-	-	WHITE	GREEN	0.080" SHEET ALUMINUM	-	-	-	-	-	7' - 0"	MOUNTED BELOW SIGN S1

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**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN**

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G13

ALL SIGNS SHOWN WITH DESIGNATIONS ARE TO BE FABRICATED AS DETAILED IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION)

THE FOLLOWING STANDARD DRAWINGS APPLY UNLESS OTHERWISE NOTED IN THE REMARKS: FLAT SHEET (T-S SERIES 10, 12, 16, 17, 19, 20); EXTRUDED PANEL (T-S SERIES 6, 9, 13, 14); WALL/BARRIER MOUNTED (T-S-21); MULTI-DIRECTIONAL BASE (T-S SERIES 23A, 23B, 23C); RAILROAD (T-S-16)

SIGN NO	LEGEND	SHEET NO	SIZE			COPY					SHIELD	ARROW	SIGN FACE			STEEL DESIGN (BREAK-AWAY)					MINIMUM VERTICAL CLEARANCE	REMARKS
			LENGTH	HEIGHT	RADIUS	BORDER WIDTH	CAPITAL	LOWER CASE	NUMERAL	SERIES			COPY	BACKGROUND	MATERIAL	SUPPORT TYPE	SUPPORT LENGTH	FOOTING	CONC. CU. YD.	REIN STEEL LBS.		
REMOVE EXISTING SIGNS																						
R1		2G9	EXIST.	EXIST.	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	ONE SIGN FACE AND POST TO BE REMOVED (SHEET 12)
R2	 R3-17	2G6	EXIST.	EXIST.	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	ONE SIGN FACE TO BE REMOVED (SHEET 9)
R3		2G6	EXIST.	EXIST.	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	ONE SIGN FACE TO BE REMOVED (SHEET 9)

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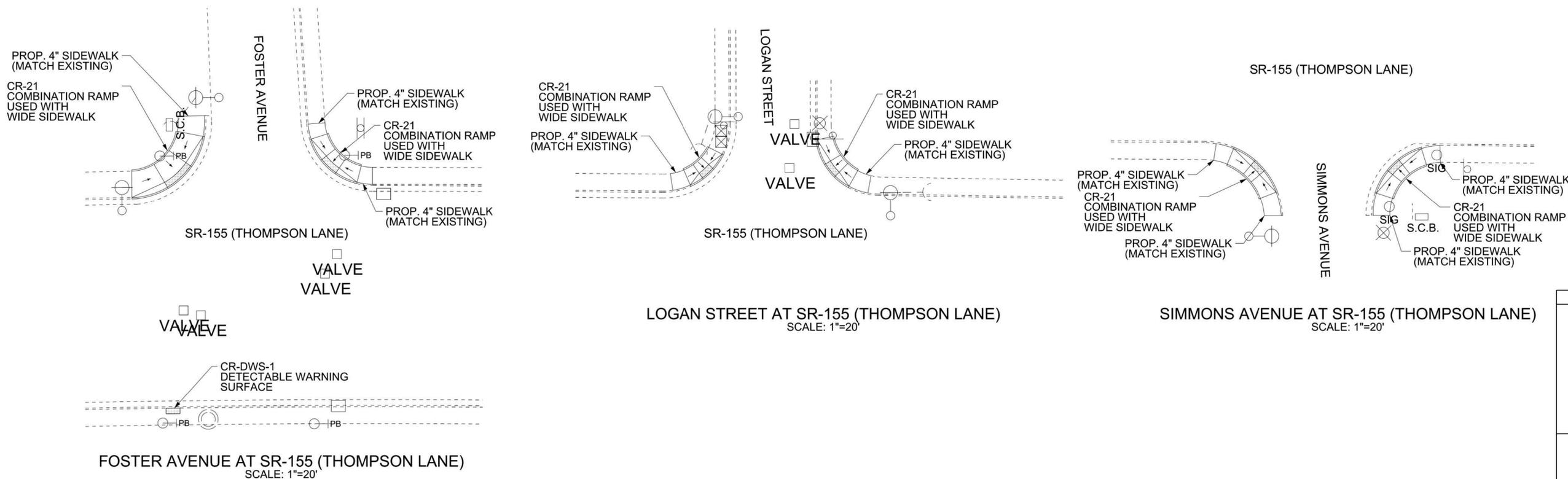
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
IMPROVEMENTS AND
SIGNING PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G14



NOTE:
 ALL UTILITY LOCATIONS SHOWN ON THESE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL FIELD LOCATE ALL UTILITIES PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR SHALL TAKE CARE TO AVOID ALL EXISTING UTILITIES.



LEGEND

	EXISTING SIGN		EXISTING GUY WIRE
	EXISTING FIRE HYDRANT		EXISTING SIGNAL CONTROL
	EXISTING WATER/GAS VALVE		EXISTING UTILITY BOX
	EXISTING MAIL BOX		DETECTABLE WARNING SURFACE
	EXISTING MANHOLE		EXISTING GUARDRAIL
	EXISTING STORM INLET		EXISTING SIGNAL POLE
	EXISTING UTILITY POLE		EXISTING PED PUSHBUTTON
	EXISTING LIGHT POLE		

TDOT REGION 3 DISTRICT 37 CURB RAMP IMPROVEMENTS
 SR-155
 LOG MILE 0.00 TO 0.13 AND LOG MILE 1.61 TO 2.80
 DAVIDSON COUNTY

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NOT TO SCALE
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
CURB RAMP IMPROVEMENTS L.M. 1.61 TO L.M. 1.98

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	2G15



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LEGEND			
	EXISTING SIGN		EXISTING FENCE
	EXISTING FIRE HYDRANT		EXISTING GUY WIRE
	EXISTING WATER/GAS VALVE		EXISTING SIGNAL CONTROL
	EXISTING MAIL BOX		EXISTING UTILITY BOX
	EXISTING MANHOLE		DETECTABLE WARNING SURFACE
	EXISTING STORM INLET		EXISTING GUARDRAIL
	EXISTING UTILITY POLE		EXISTING SIGNAL POLE
	EXISTING LIGHT POLE		EXISTING PED PUSHBUTTON

TDOT REGION 3 DISTRICT 37 CURB RAMP IMPROVEMENTS

SR-155
LOG MILE 0.00 TO 0.13 AND LOG MILE 1.61 TO 2.80
DAVIDSON COUNTY

SEALED BY
NOT TO SCALE
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
CURB RAMP IMPROVEMENTS L.M. 2.08 TO L.M 2.32

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	3

RIGHT-OF-WAY

(12) EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITY NOTES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

TELEPHONE & FIBER:

AT&T
 116 SOUTH CANNON AVE
 MURFREESBORO, TN 37129
 CONTACT: LEE KORNEGAY
 OFFICE PHONE: 615 848 2082
 Email: KK4096@ATT.COM

WATER & SEWER:

METRO WATER & SEWER
 1600 SECOND AVE NORTH
 NASHVILLE, TN 37208
 CONTACT: KECIA CAIN
 OFFICE PHONE: 615 862 4801
 Email: KECIA.CAIN@NASHVILLE.GOV

ELECTRIC:

NASHVILLE ELECTRIC SERVICE
 1214 CHURCH STREET, ROOM 363
 NASHVILLE, TN 37246
 CONTACT: JON SIPES
 OFFICE PHONE: 615 747 3529
 Email: JSIPES@NESPOWER.COM
 UTILITYRELOCATIONS@NESPOWER.COM

FIBER OPTIC:

AT&T FIBER OPTIC CABLE
 600 W PARK AVE
 GREENWOOD, MS 38930
 CONTACT: SCOTT GRANTHAM
 OFFICE PHONE: 662 392 1996
 Email: NG8314@ATT.COM

FIBER OPTIC:

CROWN CASTLE
 6325 ARDREY KELL RD, SUITE 600
 CHARLOTTE, NC 28277
 CONTACT: EMILY CATES
 OFFICE PHONE: 443 297 6220
 Email: EMILY.CATES@CROWNCastle.COM

FIBER OPTIC:

GOOGLE NETWORK
 1101 MCGAVOCK ST, SUITE 200
 NASHVILLE, TN 37203
 CONTACT: RICK BOLTON
 OFFICE PHONE: 629 888 2258
 Email: BOLTONR@GOOGLE.COM

FIBER OPTIC:

XO COMMUNICATIONS (VERIZON)
 575 HICKORY HILLS BLVD
 WHITES CREEK, TN 37189
 CONTACT: DEREK DEE
 OFFICE PHONE: 615 777 7727
 CELL PHONE: 615 207 1297
 Email: DEREK.R.DEE@VERIZON.COM

WATER & SEWER:

METRO WATER & SEWER
 1600 SECOND AVE NORTH
 NASHVILLE, TN 37208
 CONTACT: PAISLEY MAROTTA-MATHEWS
 OFFICE PHONE: 615 862 4142
 Email: PAISLEY.MAROTTA@NASHVILLE.GOV

ELECTRIC:

TENNESSEE VALLEY AUTHORITY (TVA)
 1101 MARKET STREET MR-4G
 CHATTANOOGA, TN 37402
 CONTACT: STEPHEN WILLIAMS
 OFFICE PHONE: 662 255 6272
 Email: SEWILLIAMS@TVA.GOV

CABLE:

COMCAST
 660 MAINSTREAM DR
 NASHVILLE, TN 37228
 CONTACT: KATELYN GROSS
 CELL PHONE: 615 961 2453
 Email: KATELYN_GROSS@COMCAST.COM
 NAS-NASHVILLECONSTRUCTIONBETTERMENTS@COMCAST.COM

GAS:

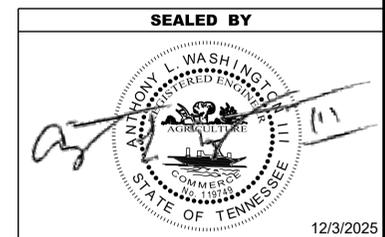
PIEDMONT GAS
 83 CENTURY BOULEVARD
 NASHVILLE, TN 37214
 CONTACT: NATE MILLER
 OFFICE PHONE: 615 872 2457
 Email: NATHANIEL.MILLER2@DUKE-ENERGY.COM

FIBER OPTIC:

GOOGLE NETWORK
 1101 MCGAVOCK ST, SUITE 200
 NASHVILLE, TN 37203
 CONTACT: JOSHUA DAY
 CELL PHONE: 615 962 4402
 Email: JOSHUADAY@GOOGLE.COM

FIBER OPTIC:

XO COMMUNICATIONS (VERIZON)
 575 HICKORY HILLS BLVD
 WHITES CREEK, TN 37189
 CONTACT: MICHAEL WEAVER
 OFFICE PHONE: 615 218 0656
 Email: MICHAEL.WEAVER@VERIZON.COM



**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

RIGHT-OF-WAY
 NOTES,
 UTILITY NOTES
 AND
 UTILITY OWNERS

UTILITY OWNERS

STREETLIGHTS & FIBER:

NASHVILLE DEPT. OF TRANSPORTATION (NDOT)
 720 SOUTH FIFTH STREET
 NASHVILLE, TN 37206
 CONTACT: MIKE HIRTZER
 OFFICE PHONE: 615 880 3261
 Email: MICHAEL.HIRTZER@NASHVILLE.GOV

FIBER OPTIC:

UNITI (FORMERLY WINDSTREAM)
 658 TREE FARM ROAD
 LINDEN, TN 37096
 CONTACT: TOMMY RAYFIELD
 CELL PHONE: 812 253 1587
 Email: TOMMY.RAYFIELD@WINDSTREAM.COM

FIBER OPTIC:

LUMEN (FORMERLY CENTURYLINK & LEVEL 3 COMMUNICATIONS)
 520 WHALEY ST
 COLUMBIA, SC 29201
 CONTACT: JOHN BOEDEKER
 CELL PHONE: 512 334 8351
 Email: JOHN.BOEDEKER@LUMEN.COM
 RELOCATIONS@LUMEN.COM

FIBER OPTIC:

LUMEN (FORMERLY CENTURYLINK & LEVEL 3 COMMUNICATIONS)
 2530 PERIMETER PLACE DR
 NASHVILLE, TN 37214
 CONTACT: JEFFREY CANNON
 OFFICE PHONE: 615 263 1128
 Email: JEFFREY.CANNON@LUMEN.COM

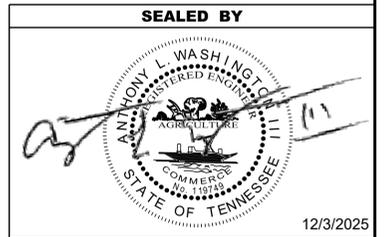
FIBER OPTIC:

ZAYO GROUP
 820 FESSLERS PARKWAY, SUITE 240
 NASHVILLE, TN 37210
 CONTACT: RICKY WIMBERLEY
 CELL PHONE: 731 571 2600
 Email: RICKY.WIMBERLEY@ZAYO.COM

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ZAYO GROUP
 4701 W Hillsborough Ave
 TAMPA, FL 33614
 CONTACT: JAKE SANSOM
 CELL PHONE: 813 763 5999
 Email: JAKE.SANSOM@ZAYO.COM
 ZAYO.RELO.TENNESSEE@ZAYO.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	3-1



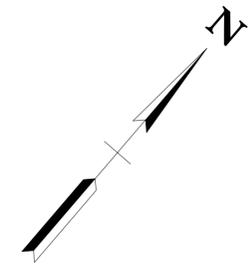
**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

RIGHT-OF-WAY
 NOTES,
 UTILITY NOTES
 AND
 UTILITY OWNERS

R.O.W. ACQUISITION TABLE																		
TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS			TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (SQUARE FEET)					
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM DRAINAGE	SLOPE	CONST	AIR RIGHTS	PERM RAILROAD
				BOOK	PAGE													
**1	CSX Transportation, Inc.																	
ACQUISITION TOTALS (ACRES)																		

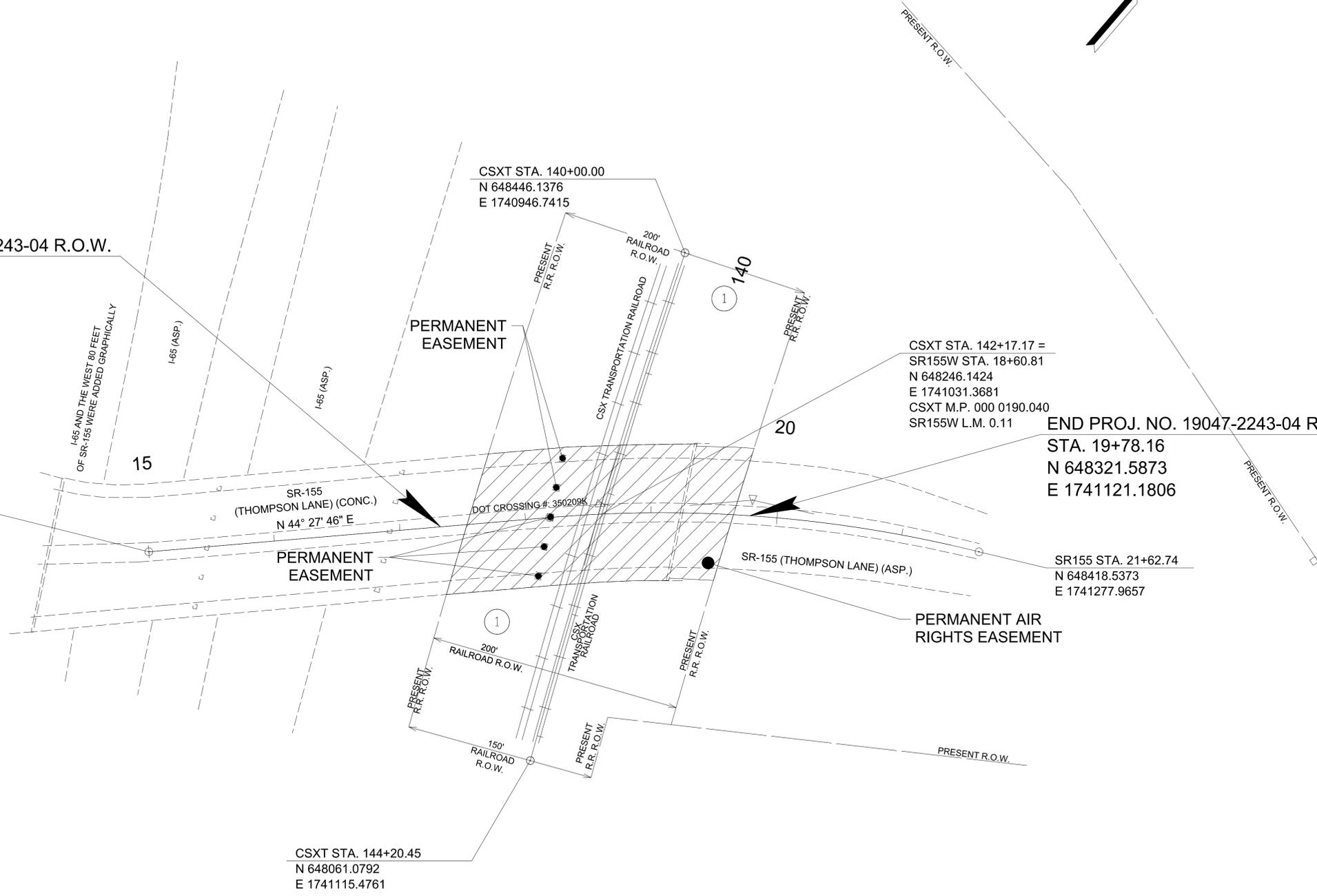
** CONFIRMATION OF EASEMENT CORRIDOR BY RAILROAD ENTITY

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	3A



BEGIN PROJ. NO. 19047-2243-04 R.O.W.
 STA. 17+32.49
 N 648155.1456
 E 1740940.9061

SR155 STA. 15+00.00
 N 647989.2185
 E 1740778.0618

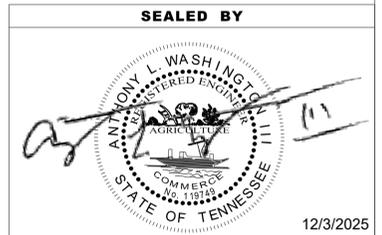


CSXT STA. 142+17.17 =
 SR155W STA. 18+60.81
 N 648246.1424
 E 1741031.3681
 CSXT M.P. 000 0190.040
 SR155W L.M. 0.11

END PROJ. NO. 19047-2243-04 R.O.W.
 STA. 19+78.16
 N 648321.5873
 E 1741121.1806

SR155 STA. 21+62.74
 N 648418.5373
 E 1741277.9657

CSXT STA. 144+20.45
 N 648061.0792
 E 1741115.4761



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00006 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

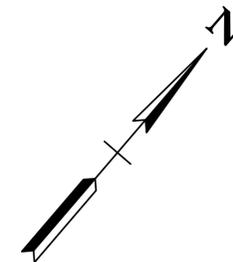
PROPERTY MAP
 AND
 RIGHT-OF-WAY
 ACQUISITION
 TABLE

LEGEND	
	PERMANENT EASEMENT
	PERMANENT AIR RIGHTS EASEMENT

NOTE: THE AGREEMENT REQUIRED FOR THE RAILROAD CROSSING WILL BE OBTAINED BY THE ROW DIVISION'S UTILITY OFFICE RAILROAD COORDINATOR THROUGH NEGOTIATIONS AND SPECIAL PROVISIONS WITH THE RAILROAD.

SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	4



BEGIN PROJ. NO. 19047-2243-04 R.O.W.

STA. 17+32.49

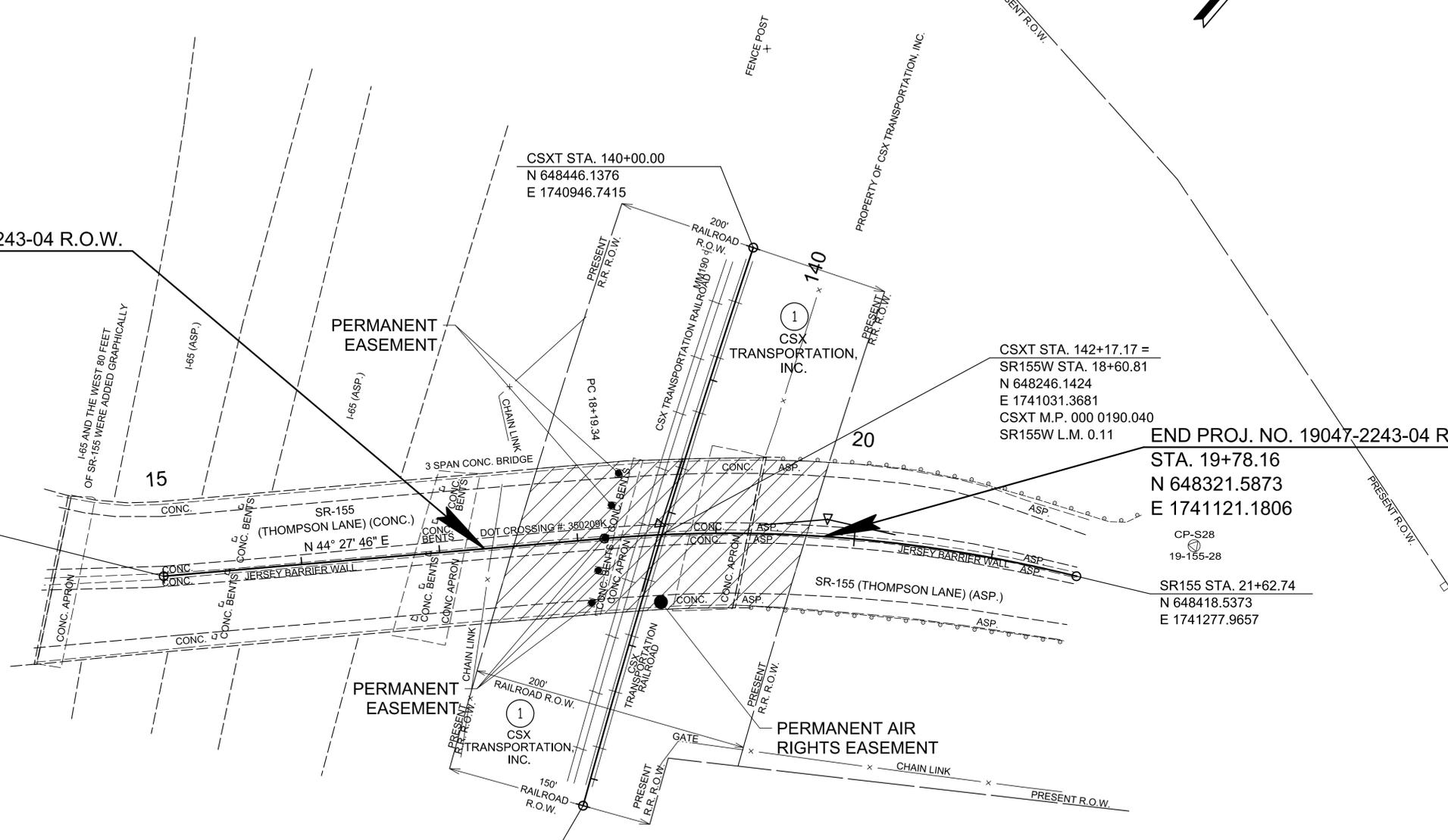
N 648155.1456

E 1740940.9061

SR155 STA. 15+00.00

N 647989.2185

E 1740778.0618



END PROJ. NO. 19047-2243-04 R.O.W.

STA. 19+78.16

N 648321.5873

E 1741121.1806

CP-S28
19-155-28

SR155 STA. 21+62.74

N 648418.5373

E 1741277.9657

CP-S29
19-155-29

CSXT STA. 144+20.45
N 648061.0792
E 1741115.4761

SR-155
PI 19+92.61
N 648,340.7979
E 1,741,123.1091
Δ 18° 52' 49" (RT)
D 5° 29' 52"
R 1,042.14
L 343.41
T 173.27
SE 0.000 FT/FT
DESIGN SPEED 00 MPH
TRANS. LENGTH 000

LEGEND	
	PERMANENT EASEMENT
	PERMANENT AIR RIGHTS EASEMENT
NOTE: THE AGREEMENT REQUIRED FOR THE RAILROAD CROSSING WILL BE OBTAINED BY THE ROW DIVISION'S UTILITY OFFICE RAILROAD COORDINATOR THROUGH NEGOTIATIONS AND SPECIAL PROVISIONS WITH THE RAILROAD.	

Point	North	East	Elevation	Station	Offset	Description
S28	648490.5673	1741328.1569	523.492	OFF CHAIN	OFF CHAIN	XCP=19-155-28
S29	648566.1941	1741680.8988	509.406	OFF CHAIN	OFF CHAIN	XCP=19-155-29
S30	647937.9752	1748076.5109	559.016	OFF CHAIN	OFF CHAIN	XCP=19-155-30
S31	647973.3640	1746386.4295	560.268	OFF CHAIN	OFF CHAIN	XCP=19-155-31

SEALED BY



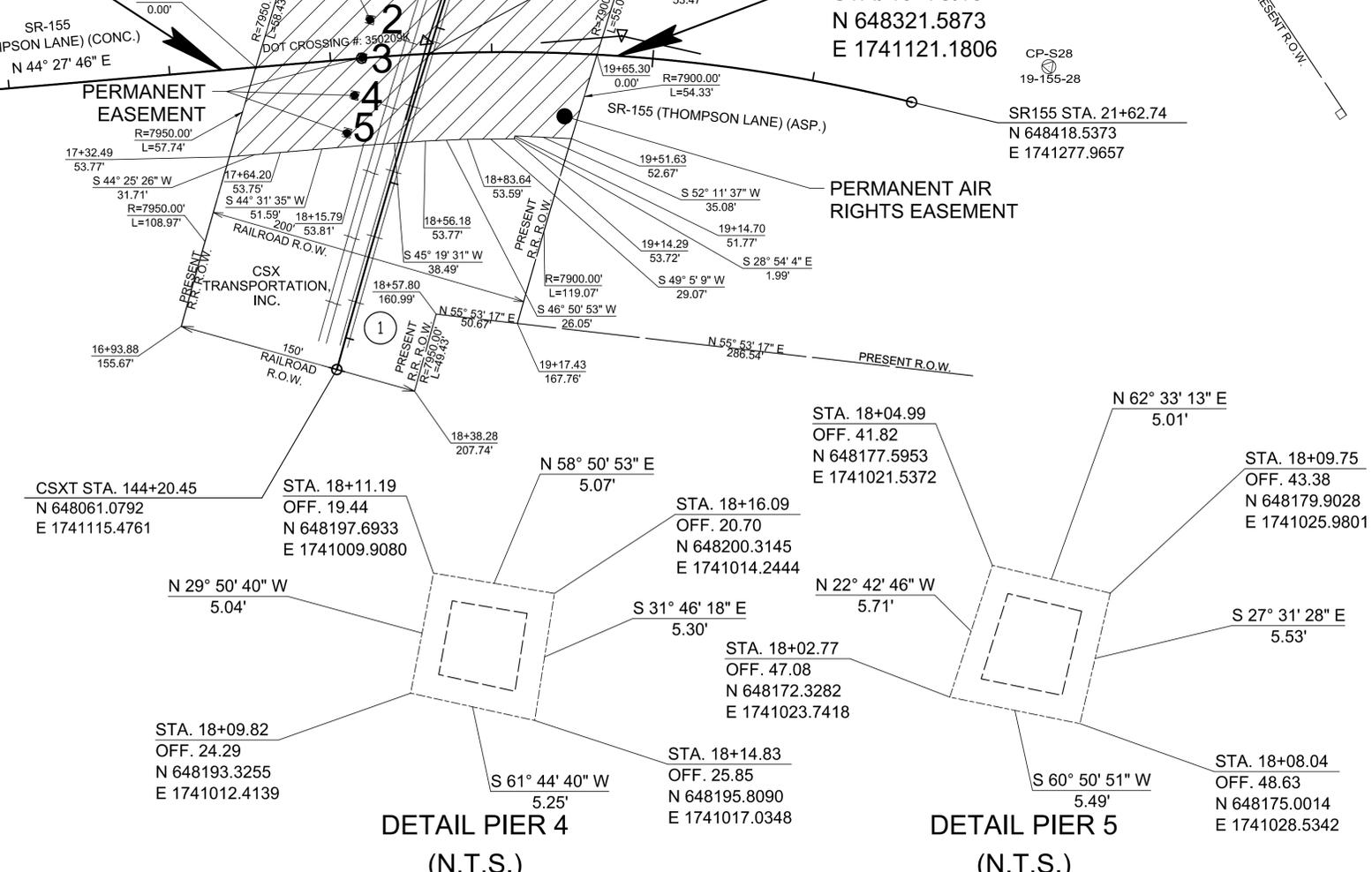
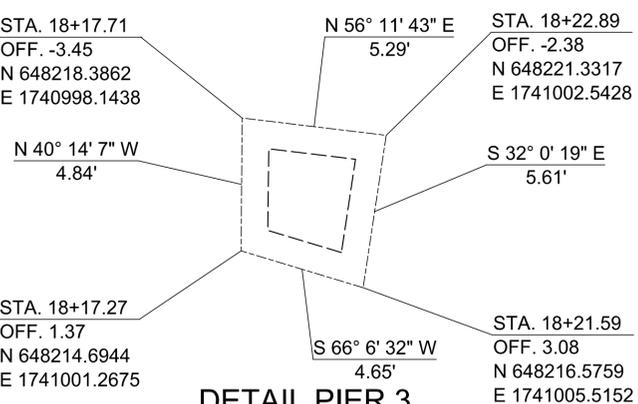
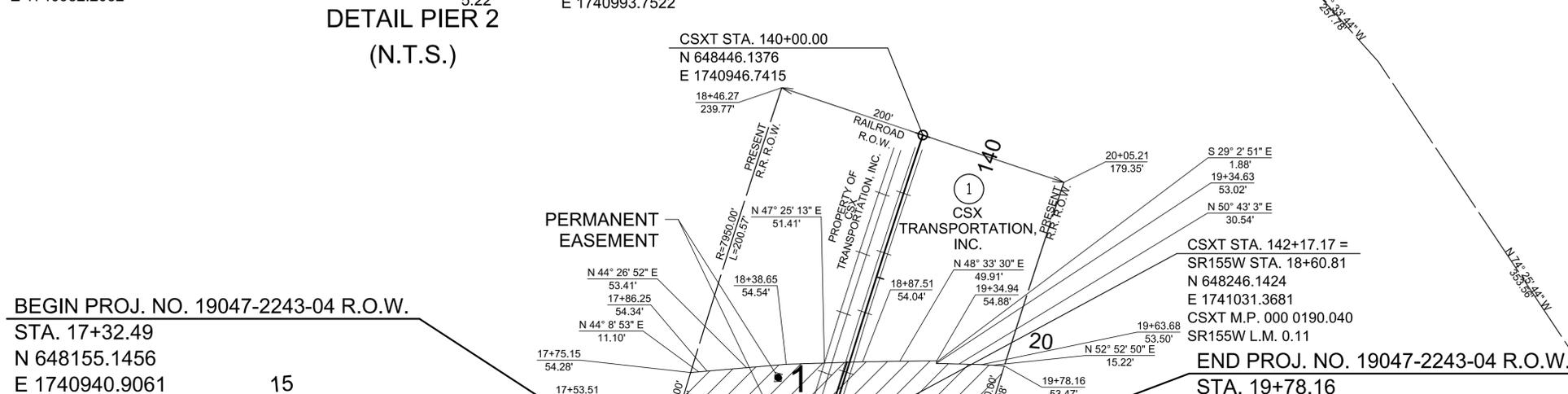
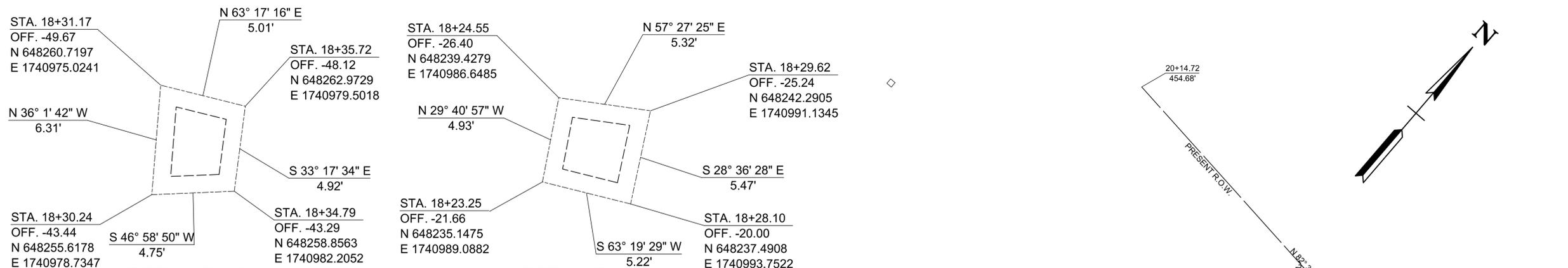
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00006 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT
LAYOUT

STA. 15+00.00 TO STA. 21+62.74
SCALE: 1"= 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	4A



LEGEND

	PERMANENT EASEMENT
	PERMANENT AIR RIGHTS EASEMENT

NOTE: THE AGREEMENT REQUIRED FOR THE RAILROAD CROSSING WILL BE OBTAINED BY THE ROW DIVISION'S UTILITY OFFICE RAILROAD COORDINATOR THROUGH NEGOTIATIONS AND SPECIAL PROVISIONS WITH THE RAILROAD.

Point	North	East	Elevation	Station	Offset	Description
S28	648490.5673	1741328.1569	523.492	OFF CHAIN	OFF CHAIN	XCP=19-155-28
S29	648566.1941	1741680.8988	509.406	OFF CHAIN	OFF CHAIN	XCP=19-155-29
S30	647937.9752	1748076.5109	559.016	OFF CHAIN	OFF CHAIN	XCP=19-155-30
S31	647973.3640	1746386.4295	560.268	OFF CHAIN	OFF CHAIN	XCP=19-155-31

CP-S29
19-155-29

SEALED BY

12/3/2025

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00006 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY
DETAILS

STA. 15+00.00 TO STA. 21+62.74
 SCALE: 1"= 50'

9/5/2025 10:34:14 AM c:\pwworking\east01\104505714104A.dgn

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2026	19S155-S8-013	T1

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.
 - a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

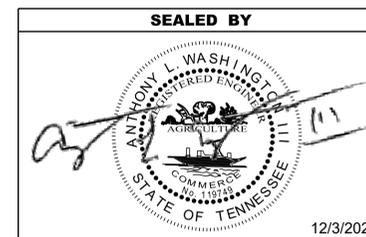
THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

PROJECT NO.	YEAR	SHEET NO.	
19S155-M3-013	2026	B-1	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
-	-		
-	-		
-	-		
-	-		
-	-		

INDEX OF DRAWINGS

DWG. NO.

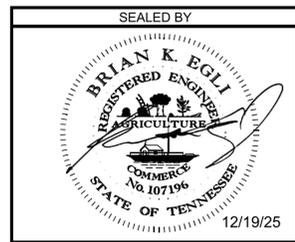
LAST
REV. DATE

SIGNATURE SHEET	BRIDGE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION, ESTIMATED QUANTITIES AND PLANS VIEW REPAIRS LOCATIONS	B2	
PHASE CONSTRUCTION	B3	
TYPE 1 THIN EPOXY OVERLAY NOTES	B4	

INDEX OF REFERENCE DRAWINGS

DWG. NO.

LAYOUT OF BRIDGE	BR-82-1
SUPERSTRUCTURE	BR-82-7
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	K-88-144

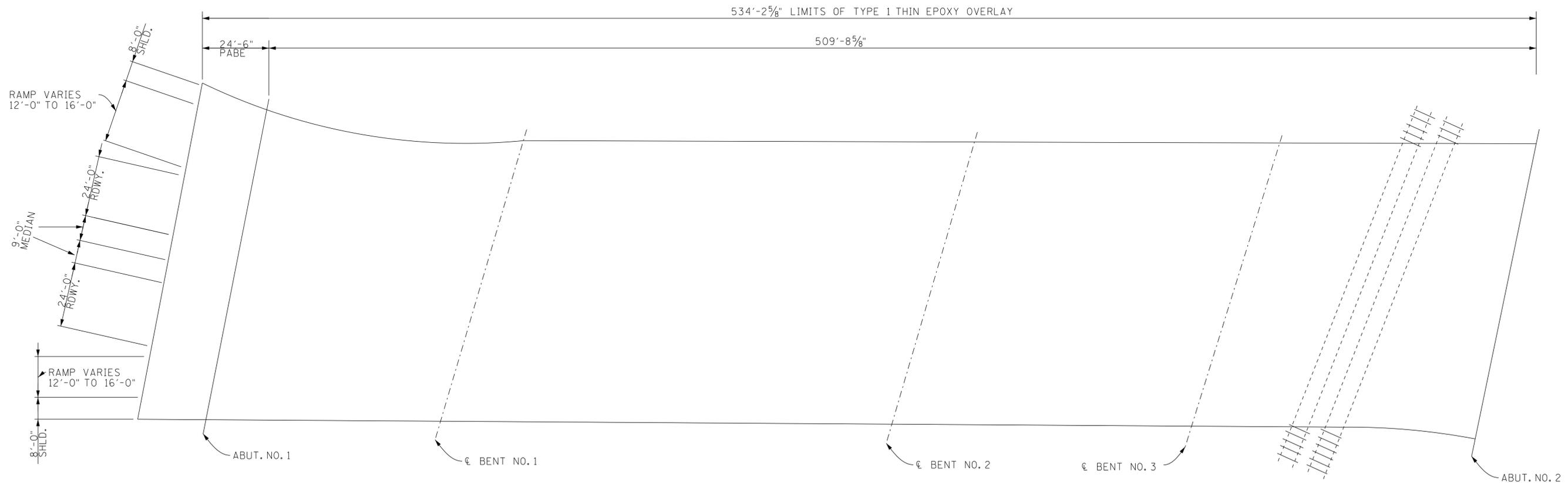


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
19-SR155-0.11
I-65 & CSX RR OVER
FED. BRIDGE ID NO.
19I00650011
DAVIDSON COUNTY
2026

PIN NO.: 129489.00
DESIGN BY: SILESHI ERGICHO DATE: 6/25
SUPERVISED BY: KEVIN MARTINKO DATE: 6/25
CHECKED BY: DATE: / /

PROJECT NO.	YEAR	SHEET NO.	
19S155-M3-013	2026	B-2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES			
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
19-SR155-0.11 OVER I-65 & CSX RR (19I00650011)	BR-82-1 BR-82-7 K-88-144	TYPE 1 THIN EPOXY OVERLAY	7627
TOTAL			7627



PLAN VIEW

19-SR155-0.11



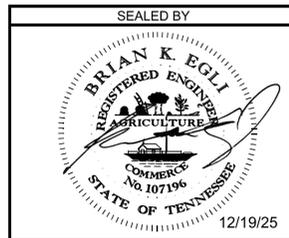
PIN NO.: 129489.00

DESIGN BY: SILESHI ERGICHO DATE: 6/25

DRAWN BY: KEVIN MARTINKO DATE: 6/25

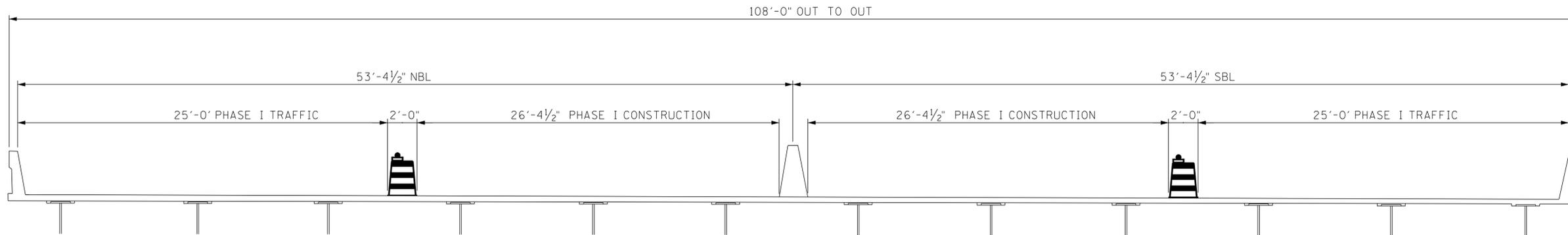
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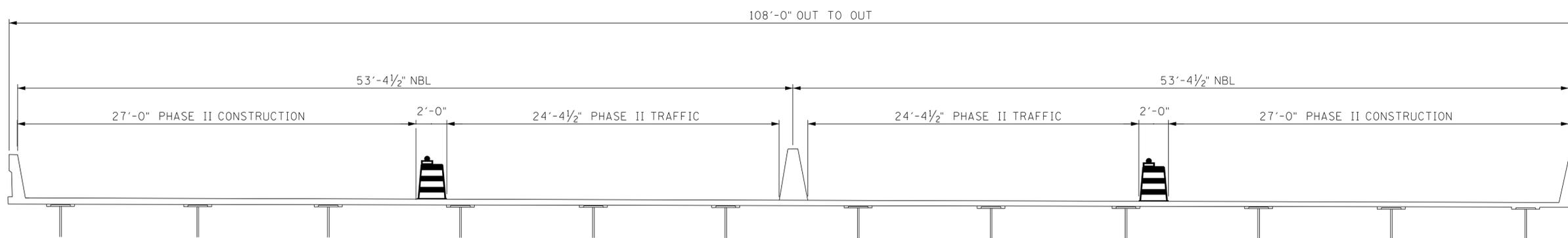


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE TABULATION,
ESTIMATED QUANTITIES
& PLAN VIEW
REPAIRS LOCATIONS
19-SR155-0.11 OVER
I-65 & CSX RR
FED. BRIDGE ID NO.
19I00650011
DAVIDSON COUNTY
2026

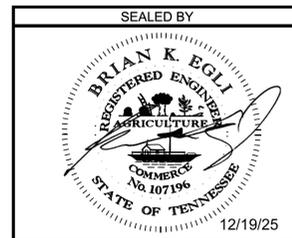
PROJECT NO.	YEAR	SHEET NO.	
19S155-M3-013	2026	B-3	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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TYPICAL SECTION (SPAN-2)
 (LOOKING AHEAD ON THE SURVEY)
 PHASE I CONSTRUCTION



TYPICAL SECTION (SPAN-2)
 (LOOKING AHEAD ON THE SURVEY)
 PHASE II CONSTRUCTION



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PHASE CONSTRUCTION
 19-SR155-0.11 OVER
 I-65 & CSX RR
 FED. BRIDGE ID NO.
 19I00650011
 DAVIDSON COUNTY
 2026

PIN NO.: 129489.00
 DESIGN BY: SILESHI ERGICHO DATE: 6/25
 DRAWN BY: KEVIN MARTINKO DATE: 6/25
 SUPERVISED BY: KEVIN MARTINKO DATE: 6/25
 CHECKED BY: DATE: / /

PROJECT NO.	YEAR	SHEET NO.	
19S155-M3-013	2026	B-4	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

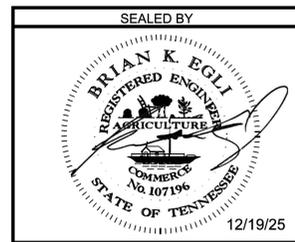
THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1 1/2" DIAMETER. THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

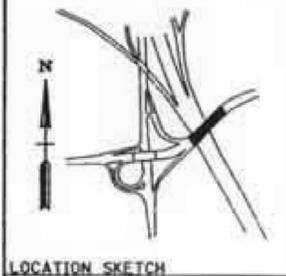
THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

**** SPECIAL NOTE:**
 THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 TYPE 1 THIN EPOXY
 OVERLAY NOTES
 19-SR155-0.11 OVER
 I-65 & CSX RR
 FED. BRIDGE ID NO.
 19I00650011
 DAVIDSON COUNTY
 2026

PIN NO.: 129489.00
 DESIGN BY: SILESHI ERGICHO DATE: 11
 DRAWN BY: SILESHI ERGICHO DATE: 6/25
 SUPERVISED BY: KEVIN MARTINKO DATE: 6/25
 CHECKED BY: DATE: 11



LIST OF DRAWINGS

TITLE	DWG. NO.	LATEST REV. DATE
LAYOUT OF BRIDGE	BR-82-1	
GENERAL NOTES	BR-82-2	
SPECIAL NOTES	BR-82-3	
EPOXY INJECTION NOTES	BR-82-4	
ESTIMATED BRIDGE QUANTITIES	BR-82-5	
PHASING	BR-82-6	
SUPERSTRUCTURE	BR-82-7	
SUPERSTRUCTURE	BR-82-8	
PAVEMENT AT BRIDGE ENDS	BR-82-9	
SUPERSTRUCTURE DETAILS	BR-82-10	
SUPERSTRUCTURE DETAILS	BR-82-11	
BILL OF STEEL	BR-82-12	

LIST OF STANDARD DRAWINGS

DRAWING NO.	LAST REV. DATE	DRAWING
STD-1-155		BRIDGE RAILING SINGLE SLOPE CONCRETE PARAPET
STD-1-2	01-05-01	SLIDER PLATE AND DECK DRAINS FOR CONCRETE PARAPET
STD-1-3SS		STANDARD SINGLE SLOPE CONCRETE MEDIAN BARRIER
STD-1-5	04-08-05	PAVEMENT AT BRIDGE ENDS
STD-8-2	04-08-05	LIGHT STANDARD SUPPORT DETAILS
STD-8-3	09-01-91	MEDIAN BARRIER LIGHT STANDARD SUPPORT DETAILS
STD-8-4		SIGN, LUMINAIRE AND TRAFFIC SIGNAL SUPPORTS
STD-9-1	12-19-94	STANDARD REINFORCING SUPPORT BAR DETAILS FOR CONCRETE SLABS
SBR-2-115	01-04-96	GENERAL NOTES AND DETAILS FOR EXPANSION JOINT REPLACEMENT CONSTRUCTION TYPES *A* THRU *J* - 1991
SBR-2-116	01-04-96	GENERAL NOTES AND DETAILS FOR EXPANSION JOINT REPLACEMENT CONSTRUCTION TYPES *A* THRU *J* - 1991

LIST OF SPECIAL PROVISIONS

NO.	LAST REV. DATE	REGARDING
105A	**	APPROVAL OF SHOP DRAWINGS
108B	**	PROJECT COMPLETION AND INCENTIVE/DISINCENTIVE
604CR	**	REPAIR OF BRIDGE DECK CRACKS
604HD	**	BRIDGE DECK PREPARATION, REPAIR AND CONCRETE OVERLAY USING HYDRODEMOLITION
604M	**	MODULAR EXPANSION JOINTS
604S	**	STRIP SEAL EXPANSION JOINTS

** DENOTES: CURRENT REVISION DATE AS PER CONTRACT DOCUMENTS
NOTE: EXISTING LIGHTS ON BRIDGE TO BE REMOVED, STORED AND RE-INSTALLED. SEE REFERENCE DRAWING M-8-15A FOR LOCATIONS OF EXISTING LIGHTING.

PROJECT NO.	YEAR	SHEET NO.
19947-4150-04	2007	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

LIST OF REFERENCE DRAWINGS

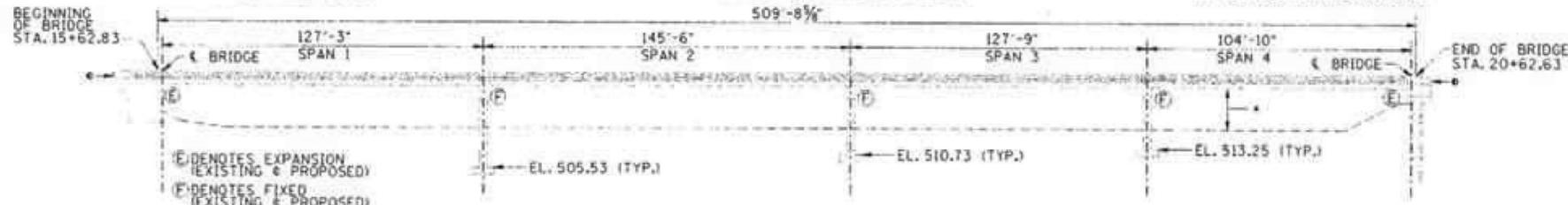
(TO BE PRINTED WITH PLANS)
DRAWING NO.
M-8-14 THRU M-8-17
M-8-21
M-8-24 THRU M-8-38

SCOPE OF WORK

1. PHASED CONSTRUCTION (SEE TRAFFIC CONTROL PLAN).
2. REMOVE AND REINSTALL EXISTING LIGHT STANDARDS & SIGN STRUCTURE.
3. REPLACE EXISTING CANTILEVERS AND BRIDGERAILS.
4. REMOVE 1" EXISTING DECK BY HYDRODEMOLITION.
5. ADD 4 1/2" CONCRETE OVERLAY.
6. REPLACE MEDIAN BARRIER RAIL.
7. GENERAL CONCRETE REPAIRS.
8. COMPLETE EPOXY INJECTION AS REQUIRED.
9. OVERLAY APPROACH PAVEMENTS.
10. PAINT EXISTING BEAMS AND BEARINGS.
11. TEXTURE COAT STRUCTURE.

NOTES

1. THE CONTRACTOR OR HIS SUBCONTRACTORS SHALL CONDUCT WORK IN SUCH A MANNER AS TO SAFEGUARD THE OPERATIONS, FACILITIES, RIGHT-OF-WAY, AND PROPERTY OF C.S.X.T. ANY AND ALL WORK AFFECTING THE ABOVE ITEMS SHALL BE GOVERNED BY C.S.X.T. STANDARDS AND SPECIFICATIONS OR OTHER APPLICABLE CRITERIA, AND SUBJECT TO THE APPROVAL OF THE C.S.X.T. DIRECTOR, RIGHT-OF-WAY CONSTRUCTION OR HIS REPRESENTATIVES SO AS TO INSURE THE SAFE OPERATION OF TRAINS, PREVENT DELAYS TO TRAINS, AND INSURE THE SAFETY OF ALL CONCERNED, THE CONTRACTOR SHALL NOT STORE MATERIALS ON C.S.X.T. PROPERTY.
2. SPECIAL NOTE TO CONTRACTOR: CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO PREVENT DEBRIS FROM FALLING ON TO THE RAILROAD RIGHT-OF-WAY. THIS SHALL BE ACCOMPLISHED BY USE OF BASKETS, NETTING, WRAPPING, SHIELD, OR SIMILARLY EFFECTIVE AND APPROVED MEANS THAT WILL NOT INFRINGE ON TEMPORARY VERTICAL CONSTRUCTION CLEARANCES IN THESE PLANS. BE ADVISED THAT C.S.X.T. NO LONGER ALLOW TRACK OR TRACKS TO BE OBSTRUCTED WITH AT-GRADE CRANE MAT PROTECTION. COMPLETE AND FULL PROTECTION OF THE CONTRACTOR WORK AREA FOR THE STRUCTURE DEMOLITION AND CANTILEVER CONSTRUCTION WITHIN ALLOWABLE CLEARANCES WILL BE REQUIRED.
3. CONTRACTOR OR SUBCONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED AND COMPREHENSIVE PLAN AND PROCEDURE FOR REVIEW AND APPROVAL BY THE DIRECTOR OF C.S.X.T. RIGHT-OF-WAY CONSTRUCTION OR HIS DESIGNATE FOR THE FOLLOWING TASKS IN THE RAILROAD SPANS: A) FULL DEPTH CONCRETE DECK, CANTILEVER OR BRIDGE RAIL DEMOLITION OR REMOVAL B) FULL DEPTH CONCRETE DECK REMOVAL IF BY HYDRODEMOLITION C) PAINT PREPARATION AND PAINTING OF SUPERSTRUCTURE D) TEXTURE COAT OF EXPOSED CONCRETE.
4. NO SCAFFOLDING OR TEMPORARY FRAMEWORK WILL BE ALLOWED THAT INFRINGE ON A TEMPORARY HORIZONTAL CONSTRUCTION CLEARANCE OF 12'-0" WITHOUT THE PRESENCE AND PERMISSION OF A RAILROAD FLAGGER. IT IS ANTICIPATED THAT FULL TIME FLAGGING WILL BE REQUIRED FOR ALL ASPECTS OF THE DECK AND PIER REPAIRS.

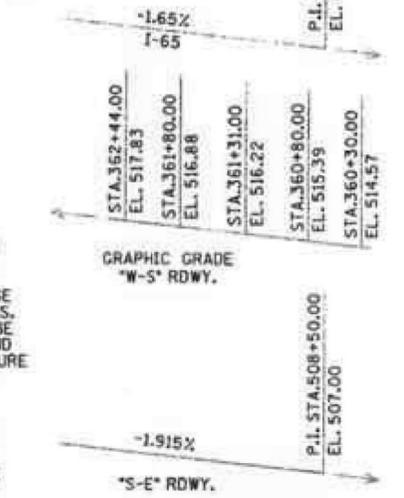


ELEVATION AT RIGHT ANGLES TO & SURVEY THOMPSON LANE

SCALE: 1"=30'-0"

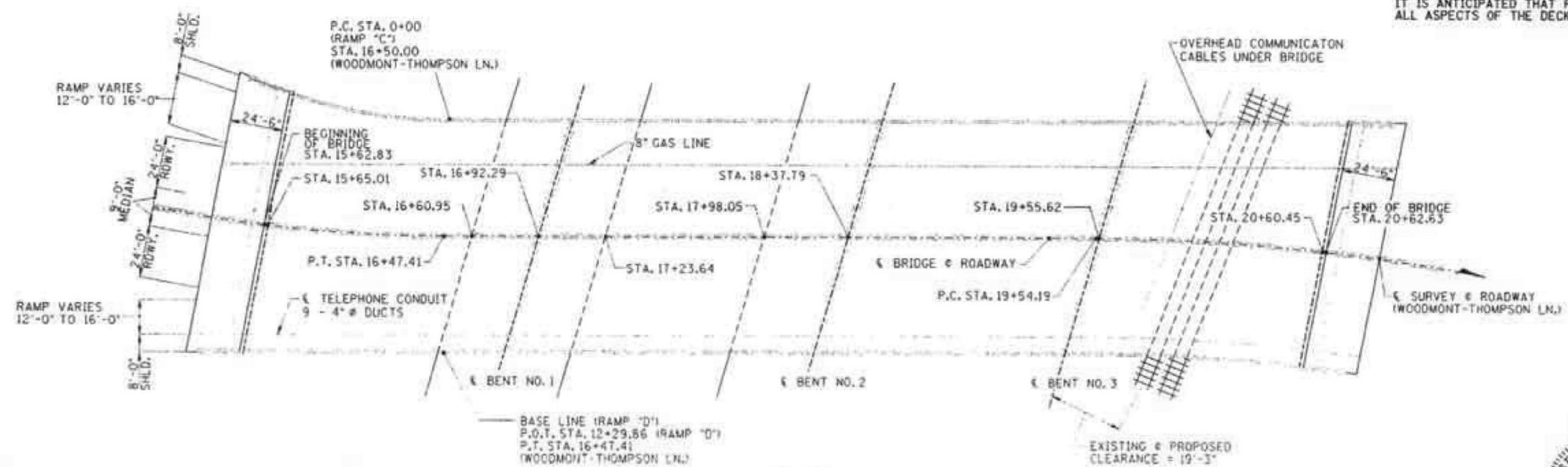
* DENOTES: EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING BRIDGE RAIL/WINGPOST CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST, ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

NOTE: CONTRACTOR SHALL MAINTAIN A MINIMUM TEMPORARY VERTICAL CLEARANCE OF 21'-6" IN THE SPAN OVER THE RAILROAD AND THIS TEMPORARY VERTICAL CLEARANCE WILL BE ENFORCED AT THIS LOCATION.



EXISTING GRADE SKETCH

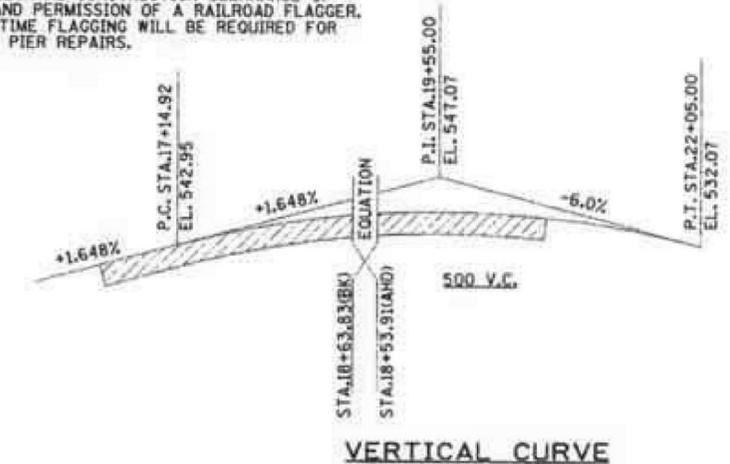
NOTE: PROPOSED GRADES ARE BASED ON EXISTING GRADES PLUS NET 3 1/2".



PLAN

SCALE: 1"=30'-0"

SEE REF. DWG. M-8-15A FOR LIGHT STANDARD AND SIGN LOCATIONS.



VERTICAL CURVE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGE
TO BE REPAIRED
BRIDGE NO. 19-SR155-00.11
THOMPSON LN. (FAU 3250)
OVER
I-65 @ CSX RR
DAVIDSON COUNTY
2007

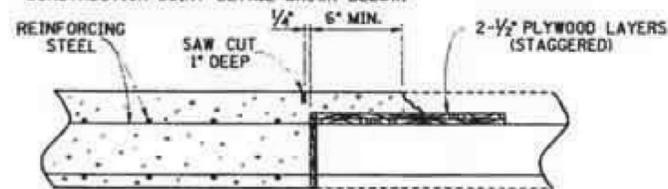


DESIGNED BY: GRESHAM SMITH & PARTNERS	DATE: JAN 2006	
DRAWN BY: E.M. STELLMAN	DATE: JAN 2006	
CHECKED BY: J.D. BREW	DATE: JAN 2006	
IN CHARGE: T.A. KNIATNYEV	DATE: JAN 2006	

NOTE: STATIONS AND DIMENSIONS TAKEN FROM EXISTING PLANS

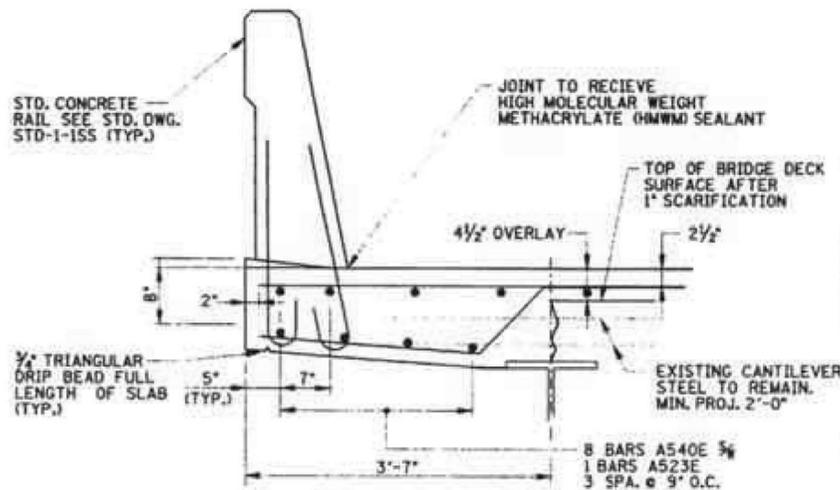
NOTE: SPAN LENGTHS ARE GIVEN ALONG THE & OF STRUCTURE

ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.



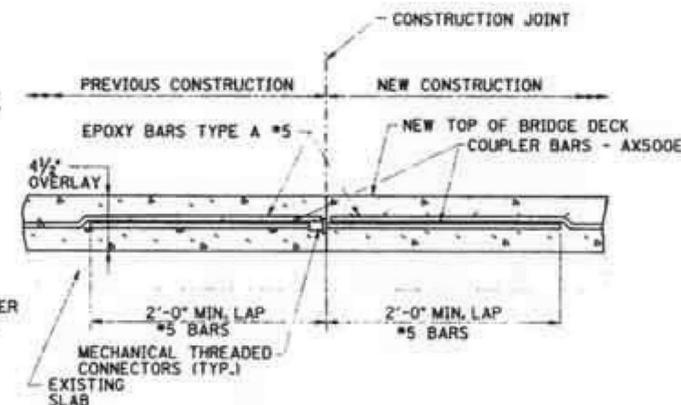
SLAB CONSTRUCTION JOINT DETAIL
(N.T.S.)

POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINTS MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED.



CANTILEVER DETAIL (TYP.)

SCALE: 1" = 1'-0"



COUPLER BAR DETAIL

SHOWING LONGITUDINAL REINFORCING STEEL SPLICING WITH MECHANICAL THREADED CONNECTORS

PROJECT NO.	YEAR	SHEET NO.
19947-4150-04	2007	

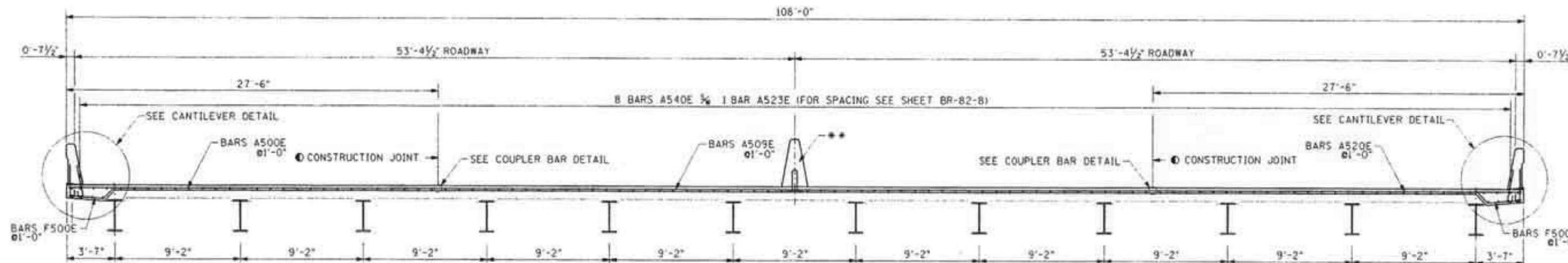
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: ALL SLAB STEEL SHALL BE EPOXY COATED PER THE STANDARD SPECIFICATIONS.

ESTIMATED QUANTITIES*

CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	REINFORCING STEEL (REPAIRS) LBS.
927	148,163

* NOTE: INCLUDES QUANTITIES FOR APPROACH SLAB OVERLAY.



TYPICAL CROSS SECTION (SPAN 2)

SCALE: 1/4" = 1'-0"
(LOOKING FORWARD ON SURVEY)
(FOR BAR DESIGNATION SEE SHEET BR-82-08)
■ DENOTES: FIRST CUT BAR
● DENOTES: CONTINUOUS BAR
⊙ DENOTES: JOINT TO RECEIVE HIGH MOLECULAR WEIGHT METHACRYLATE (HMWM) SEALANT.

** FOR MEDIAN REINFORCING, SEE STD. DWG. STD-1-355.

NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
2. THE COST OF MECHANICAL THREADED CONNECTORS AND THE COST OF THREADING THE COUPLER BARS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-10.18 REINFORCING STEEL (REPAIRS), LBS. THE NUMBER OF #5 BAR MECHANICAL COUPLERS REQUIRED IS 954.
3. FOR STANDARD REINFORCING DETAILS OF PARAPET SEE STD-1-155.
4. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET AND MEDIAN BARRIER. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.
5. THE COST OF CONCRETE REQUIRED FOR THE NEW BRIDGE DECK SLAB SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-10.85 CLASS "D" CONCRETE (OVERLAY), C.Y.
6. ALL REINFORCING STEEL FOR THE NEW CONCRETE SLAB AT SHALL BE EPOXY COATED. REINFORCING STEEL TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-10.18 REINFORCING STEEL (REPAIRS), LBS.
7. THE COST OF REMOVING AND DISPOSING OF THE EXISTING CONCRETE SLAB, MEDIAN BARRIER AND PARAPETS WITHIN THE LIMITS SHOWN SHALL BE INCLUDED IN ITEM NO. 202-04.01. REMOVAL OF STRUCTURES, L.S.
8. FOR LONGITUDINAL REINFORCEMENT DETAILS, SEE SHT. BR-82-08.

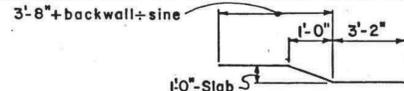
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
BRIDGE NO. 19-SR155-00.11
THOMPSON LN. (FAU 3250)
OVER
I-65 @ CSX RR
DAVIDSON COUNTY
2007



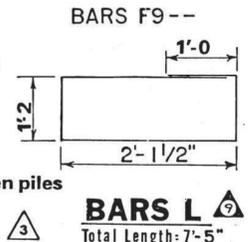
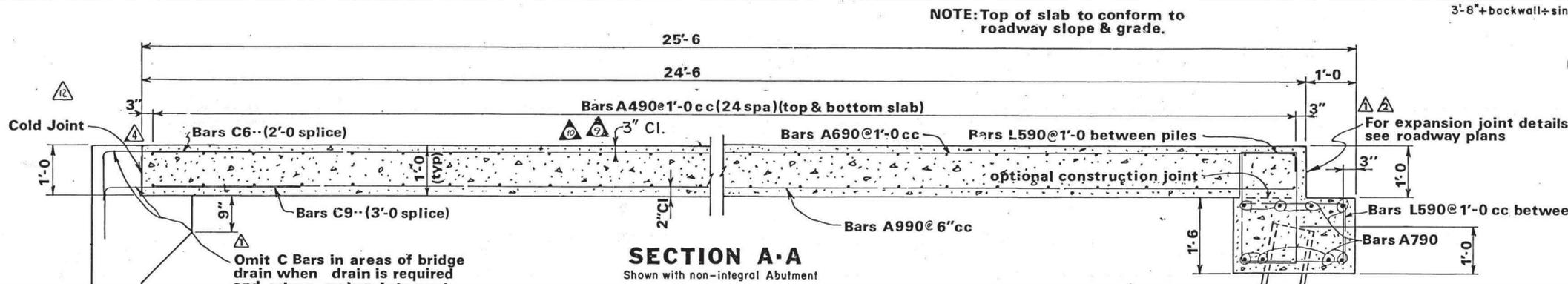
DESIGNED BY	GRESHAM SMITH & PARTNERS	DATE	JAN 2008
DRAWN BY	C.M. STELMAN	DATE	JAN 2008
CHECKED BY	J.D. BROWN	DATE	JAN 2008
CHECKED BY	T.A. KNIADZYTCZ	DATE	JAN 2008



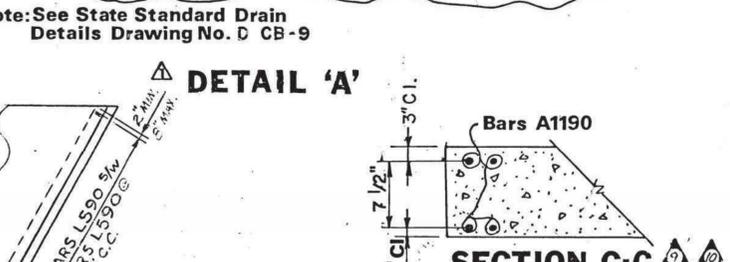
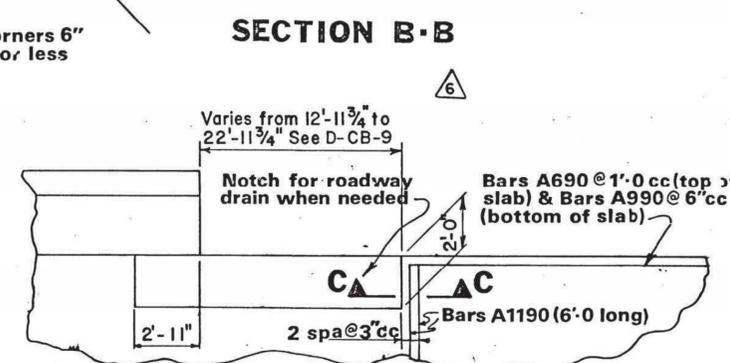
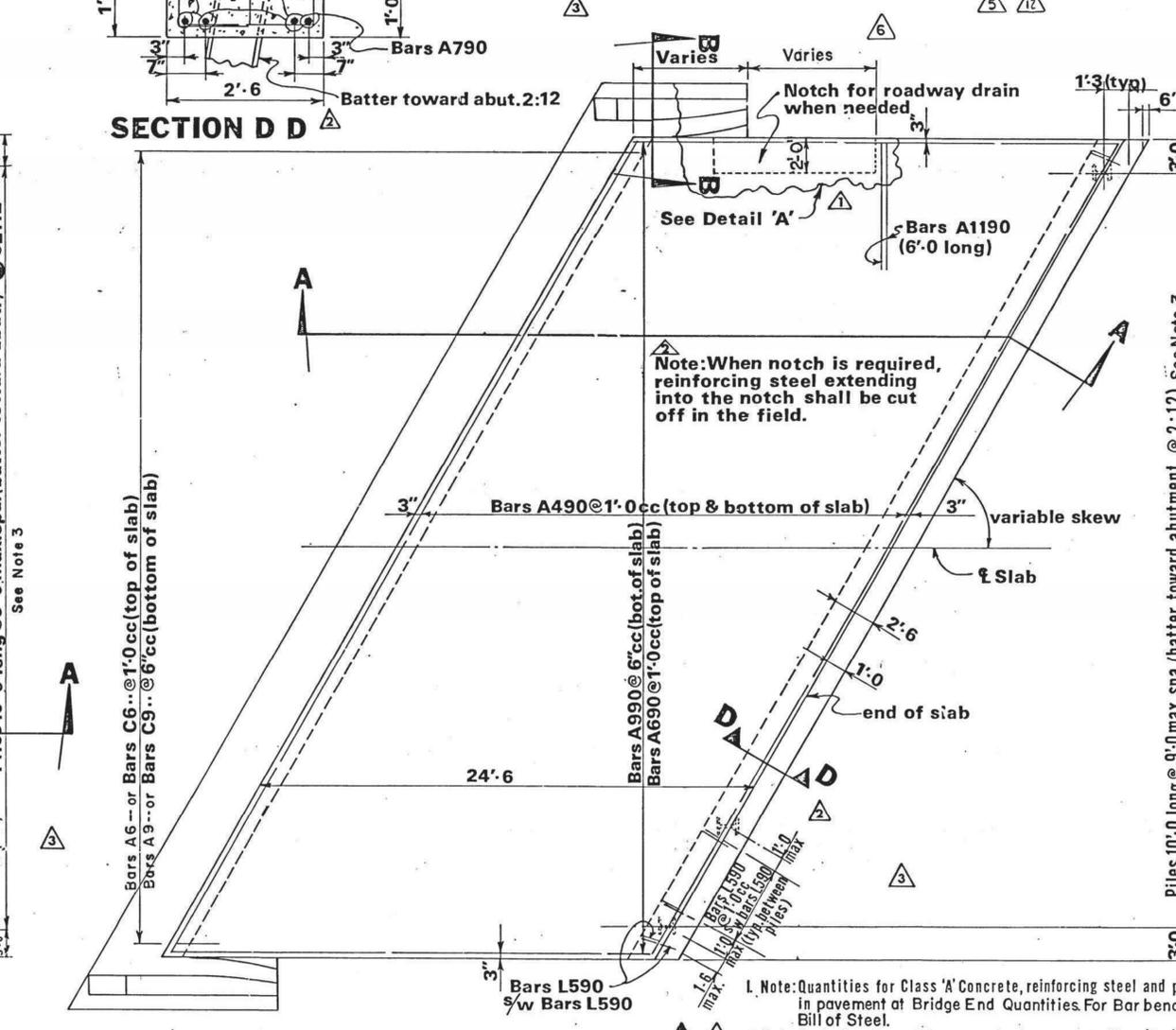
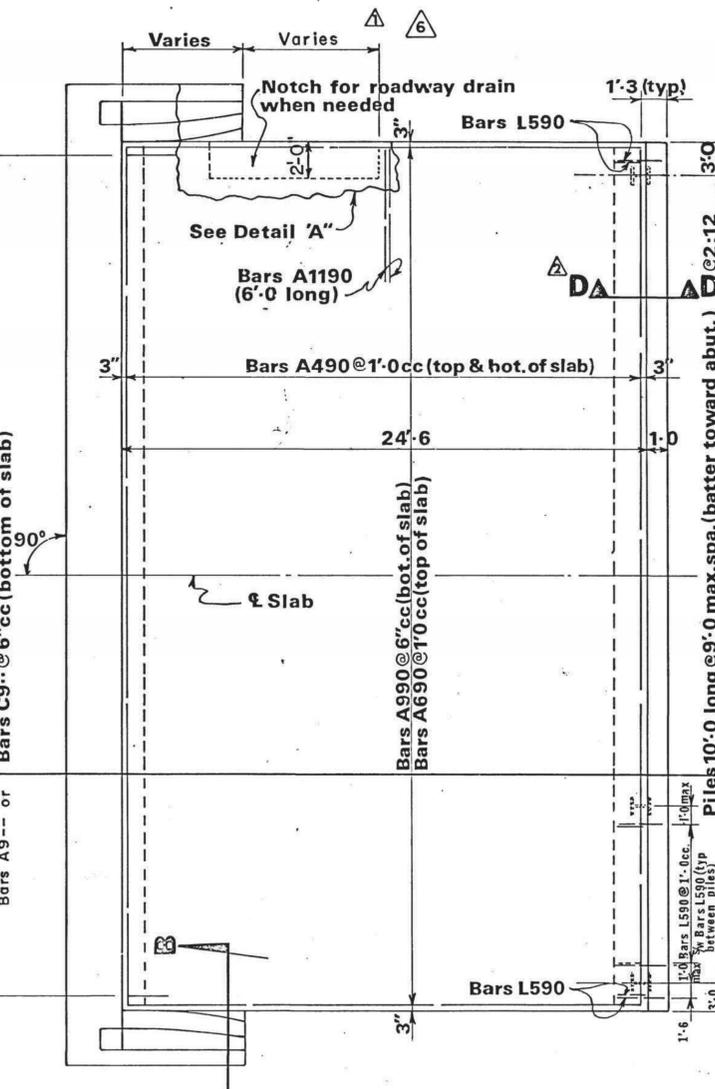
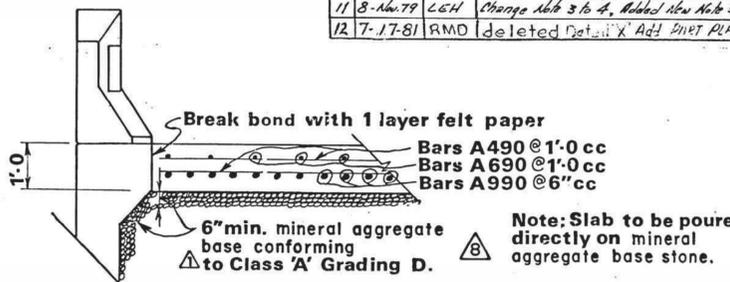
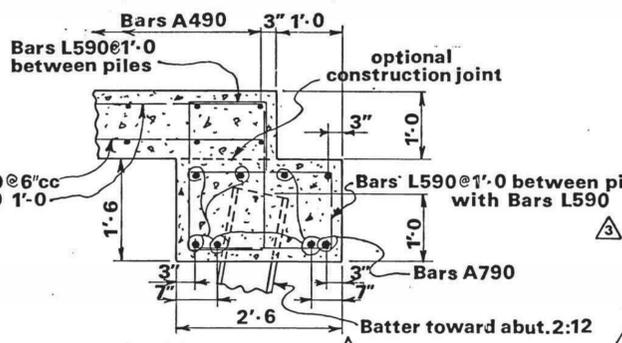
NOTE: Top of slab to conform to roadway slope & grade.



PROJECT NO.	YEAR	SHEET NO.	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	11-16-70	CMH	Compression seal added, aggregate size changed. General notes revised & bridge drain details.
2	12-18-70	CMH	Section D-D & notes added, compression seal note removed & Expansion joint note added.
3	8-12-71	RMD	Clarified spacing & no. of L590 bars
4	3-12-74	RMD	Rcmvcd V Notch
5	7-18-75	RMD	Added Detail X & Revised Note 3
6	8-18-75	RMD	Revised Roadway Drain
7	3-2-76	CPR	Changed Dwg. No. on Detail 'A'
8	6-17-76	CMH	Revised aggregate designation
9	8-30-76	RMD	Revised minimum clearances
10	5-Dec-77	EPW	Rev. Note 2 & Min. Cl. on Top Re-bar
11	8-Nov-79	LGH	Change Note 3 to 4, Added Note 3
12	7-17-81	RMD	deleted Detail X Add SHEET PLAN



Note: Location of notch for bridge drain shall be as shown on the Roadway Plans and/or Bridge Plans and shall be constructed in accordance with the fit-up requirements of Standard Drawing D-CB-9 or as directed by the Engineer



GENERAL NOTES
 CONCRETE: To be Class 'A' (F_c=3,000 psi)
 REINFORCING STEEL: To be ASTM A615. Bending dimensions shown are based on Grade 40. Spacing dimensions are center to center unless otherwise noted.
 SPECIFICATIONS: Standard Road & Bridge Specifications of the Tennessee Department of Highways (Current Edition)

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
STANDARD DRAWING
REINFORCED CONCRETE
PAVEMENT AT BRIDGE ENDS

- Note: Quantities for Class 'A' Concrete, reinforcing steel and piles are included in pavement at Bridge End Quantities. For Bar bending dimensions, see Bill of Steel.
- Note: Cost of mineral aggregate base quantity to be paid as Item 303-01 Mineral Aggregate Class A, Grading D.
- Note: In lieu of the Class A, Grading D material shown, Class B, Grading C or D may be used.
- Note: Piles shall be HP10 @ 42" or Precast Concrete Size I as shown in Estimated Quantities. Piles shall have a maximum length of 10'-0" regardless of bearing and shall be spaced at 9'-0" maximum. Piles shall be omitted if beam is supported on rock or rock fill or if the Abutment is integral.

DESIGNED BY: C.M. Hiles
 DRAWN BY: G.P.M.
 SUPERVISED BY: C.M. Hiles
 CHECKED BY: D.W. Fortner

DATE: 8-70
 DATE: 10-8-70

CORRECT: *[Signature]*
 ENGINEER OF STRUCTURES
 APPROVED: *[Signature]*
 DIRECTOR OF HIGHWAYS

K-86-144

Copy - 1 OF 4 2-5-76